



Public Art Advisory Board Agenda
For more information go to www.parkcity.org

Date: Monday, January 9, 2023

Meeting Place: Marsac Executive Conference Room, 445 Marsac Avenue, Park City, UT 84060

Time: 5:00 p.m. to 7:00 p.m.

Electronic Participation:

<https://us02web.zoom.us/j/86793650224?pwd=Z0V6VU8wa3NjL3d4M0xPbJPMHVVaQT09>

Present:

Absent:

Minutes: Stephanie Valdez, Administrative Analyst

Next Meeting: Next meeting is Monday, February 13.

*January 12 Council meeting is tentatively scheduled for Wildlife and Mining Legacy Artwork Proposal approval to move forward with RFP.

Topic 1: Call Meeting to Order (5:00 p.m.)

Motion to Call Meeting to Order:

Meeting Called to Order at:

Announce board member attendance:

Topic 2: Approve Minutes from December Meeting (5:05 p.m.)

Changes:

Motion:

Topic 3: Public Comment: Any Items Not on the Agenda (5:10 p.m.)

Notes: Remind members of the public to sign in and state full name.

Topic 4: Review Strategic Plan (5:10 p.m.)

Person: Jenny Diersen

Purpose: Review Strategic Plan.

Allocated Time: 30 minutes

Action Requested: Take a formal vote on projects as outlined in the Strategic Plan.

(1) Board discussion and questions and review Strategic Plan

(2) Open for Public Comment

(3) Board vote on donation

Attachments: Refer to Strategic Plan

Draft Recommendation:

Approve 2023 Strategic Plan

Topic 5: Staff / Council / Board Communications (5:40 p.m.)

Allocated Time: 10 minutes

Arts Council Update: Jocelyn

Summit County Public Art Update: Jocelyn/Jenny

2023 Meeting Dates:

January 9, February 13, March 13, April 17, May 8, June 12, July 10, August 14, September 11, October 16, November 13, December 11

Topic 6: Budget & Project Updates (5:50 p.m.)

Person: Jenny Diersen, Staff Liaison

Purpose: Discuss PAAB Budget and Project Updates.

Allocated Time: 20 minutes

Product: Next steps from the Board.

Action Requested:

- (1) Discuss the budget & review project updates.
- (2) Board Questions /Comments and Discussion
- (3) Open for Public Comment
- (4) Board final discussion and summary/direction.

Attachments: Refer to Budget and Project Updates

Meeting is estimated to adjourn at 6:10 p.m.

MINUTES



Public Art Advisory Board MINUTES DRAFT

Date: Monday, December 12, 2022

Meeting Place: Marsac Executive Conference Room, 445 Marsac Avenue, Park City, UT 84060

Time: 5:00 p.m. to 7:00 p.m.

Electronic Participation:

<https://us02web.zoom.us/j/86793650224?pwd=Z0V6VU8wa3NjL3d4M0xPblJPMHVhQT09> **Present:** Pam Bingham, Joann Askins, Elsa Gary, Jenny Diersen, Jocelyn Scudder, Tana Toly, Sam Osselaer, Jennifer Gardner, Lara Carlton

Absent: David Nicholas, Hillary Gilson (no longer on board due to re location)

Minutes: Stephanie Valdez, Administrative Analyst

Next Meeting: Next meeting is Monday, January 9.

Topic 1: Call Meeting to Order (5:00 p.m.)

Motion to Call Meeting to Order: Lara motioned to call the meeting to order at 5:05pm

Meeting Called to Order at: 5:05PM

Announce board member attendance: See above.

Topic 2: Approve Minutes from October Meeting (5:05 p.m.)

Changes: No changes to October Minutes

Motion: Lara moved to approve October Minutes. All in favor to approve October Minutes.

Topic 3: Public Comment: Any Items Not on the Agenda (5:10 p.m.)

Notes: Remind members of the public to sign in and state full name.

Jody is present for Public Comment, no public comment at this time.

Topic 4: Consider Artwork Donation from Save People Save Wildlife (5:10 p.m.)

Presentation by SPSW

Jenny Diersen introduced Lorlei from Save People Save Wildlife. SPSW came to Council two months ago with a proposal for Artwork; they proposed to donate two elk to be placed by the McPolin Barn, refer to packet. Jenny had sent out a scoring sheet for the Artwork to the board, average score of 13.2 out of 15.

Lorlei presented their proposal along with Tom Parkes. SPSW wishes to donate two of their elk silhouettes to Park City Public Art Collection, they call their pieces "Our Elk Herd". SPSW is a registered nonprofit that began 2015, their mission is to reduce or eliminate vehicle wildlife collisions. SPSW and UDOT have been working together to address the wildlife collisions on SR-224, Lorlei stated that SR-224 is a number one hot spot for vehicle wildlife collisions in Utah according to research and data. The worse location on SR-224 is on mile marker 7.5 near the McPolin Barn, high elk (wildlife) crossings in this area.

SPSW proposes the addition to the Elk Art to Park City's Art collection to raise awareness of Elk herd crossings on SR-224 and alert drivers of their responsibilities to avoid vehicle collisions.

Board discussion and questions and review of compiled scores

Are monthly collisions published?

Tom answered that data within the last 10 years has been compiled along with the dollar amount of damages including property damages and injuries.

Jennifer Gardner asked where the Elk Structures will be located? Same side or one either side? Tom answered that the Elk silhouettes will be located one side of the highway and they are reflective which will be visible outbound/inbound. Having the two Elk together will give drivers the idea that there is a “herd”.

Pam Bingham stated there is a concrete building already in place. Jenny replied, herself, along with Heinrich Deters, Trails & Open Space Manager and Chief Building Official and City Engineer went to look at the locations. Recommendation is to place the herd where the existing concrete pad is located. Jenny stated that there are setback requirements, that need to be followed. Next step to follow up with City Engineer.

Tom added the art pieces will need to be on an area that is level to ensure the elk are in good position. The objective is to have the herd as close to the road as possible with following all the legal requirements as needed. Tom agreed that concrete pad may be the right spot for the elk. If there is another spot for installation, SPSW will cover the cost for installation of the elk herd pieces.

How reflecting and how distracting may the elk be? Lorlei states that the reflective material is the same as what UDOT uses. In addition, Jenny states no rise of concerns from the City Engineer and other departments.

Jennifer Gardner asked If its possible to place an electric message board in between the elk, stating wildlife crossing?

Jenny responded that there are two VMS boards on both sides of the highway stating wildlife crossing. As of right now, that would be a decision for City Engineering department and UDOT for any additional VMS boards. Lorlei added that UDOT will be making the VMS boards permanent.

Lara asked how to maintain the reflective material on the elk? As stated on the packet maintenance is every seven years.

Lorlei replied that the artist stated it may never need to be maintained, depending on where the elk herd is located.

Moderate touch ups every few years. Jenny included the City would have to cover the maintenance and to keep record of the artist to refer to.

Open for Public Comment- no comment

Board vote on donation

The board motioned to approve the artwork, all were in favor to approve the SPSW donation

Next Steps: Jenny included that the project will go to Council January 5th for Council to accept the donation. Once weather permits, installation will take place in the spring.

Topic 5: Resolution for Electronic Meetings & Open Public Meeting Act Training (5:30 p.m.)

(1) Summary from staff on Electronic Meeting procedure and OPMA

Jenny presented to the PAAB the required training in the PAAB packet. Jenny stated rules have changed at the State regarding electronic meetings, PAAB must approve a new resolution for electronic meetings for the 2023 year. Per the policies, the PAAB acquires a majority voting for projects that are valued over 25k including donations that are over 25k.

(2) Jenny stated that moving forward to have six members physically present when voting and having two participating electronically when voting with projects over 25k; in person meetings is preferred. With any projects under 25k to have 5 members physically present and others can participate electronically. When there are no projects to vote on, to have two members at the anchor location and others virtually.

(3) Board Discussion

Jennifer Gardner stated that pre covid a board member cannot miss more than two meetings and asked what is the

attendance requirement? Jenny replied that the attendance requirement is the same and Jennifer's attendance is virtual, it still counts. Jenny mentioned that if this meeting was set in a later month, that voting on a project such as the Elk Herd, it would not be able to move forward due to not having enough board members on site (in person) to vote.

No Public comment at this time

(4) Board vote on locations for the project.

Lara motioned to approve Electronic Meeting Resolution 2023, all in favor.

Attachments: Refer to the draft Electronic Meeting Resolution. Draft

Recommendation:

Approve the Electronic Meeting Resolution

Topic 6: Staff / Council / Board Communications (5:40 p.m.)

Arts Council Update: Jocelyn

Jocelyn stated for the month of December the Arts Council presented the Makers Market at Create PC which highlights local artists and makers, a program that has been running for the last few months. Maker's Market has been the main activation for December.

Summit County Public Art Update: Jocelyn/Jenny

The Arts Council helps administer and manage the Summit County Public Art Board, there is RFP for the Legends Event Center in Coalville. The RFP was initially released in 2019 but funds were frozen due to Covid, funds are now unfrozen and released the RFP.

The theme is History and Heritage and a focus on Coalville, Utah. The deadline is January 13th; finalist will be selected January 17th.

Dates are being put together for a traveling exhibit with the Summit County Art collection that is housed at the Summit County courthouse. This helps get the collection out to the community.

Joceyln stated that a video of the Hoytsville mural was released, she will distribute video to the PAAB.

Review and approve 2023 Meeting Dates:

January 9, February 13, March 13, April 10 (*this is during Spring Break), May 8, June 12, July 10, August 14, September 11, October 9 (*this is Indigenous People's Day), November 13, December 11

Majority of the PAAB agreed to move April 10th to April 17th and October 9th to October 16th. Jenny will send calendar invites to all members.

Jennifer Gardner wanted to keep the April 10th meeting.

Tana Toly wanted to update the PAAB, that Monday nights are a bit challenging to attend during the Winter months, January, February, and March. Tana stated that she will send updates or have Ryan Dickey attend in her place.

Tana added the launch of Richardson Flat bus route which launched today December 12th, 2022. This bus route includes Richardson Flat to PC Heights and stop at the High School then the Transit Center. This bus route will run every 20 mins, as of right now its slated to go to 6AM-1PM.

Tana wanted to remind the PAAB rules of the road for Sundance and if any permitting that needs to be done to do so now. In addition, daytime paid parking on China Bridge has been implemented, the first 3 hours is \$1 and \$30 for the 4th hour. This will help reduce the overflow traffic from the resorts and ensure that the parking is for merchants are those visiting the stores and restaurants.

Tana wanted to give reminder to sign up for the Park City alerts, this keeps residents and nonresidents an update on what's going on in the City. Tana added the Snow Globe Stroll is still on going and til the first Sunday of January. The Today Show is filming on Main St. to show up at 6 AM to participate. Santa will be on Town Lift on Saturday.

Topic 7: Budget & Project Updates (5:50 p.m.)

(1) Discuss the budget & review project updates.

No changes in Budget as of right now. Jenny stated the projects that have been repaired include Tunnel under Bonanza Dr, McPolin Barn Tunnel and Lucy the Moose. Jenny stated that School of Fish needs welding repairs and has contacted the Artist. Jenny added that she needs to follow up with the archivist to include a few more pieces photographed and put into the collection.

Projects that are slated for 2023 are the utility boxes, which more than 40 utility boxes have been identified, next step to get an RFP going. The other 2023 projects include Neighborhood Project and Daly West Sculpture on Rail Trail. The Daly West Sculpture will go to Council January 5th for approval, once approved to begin the RFP process.

Jenny followed up with the City Engineer regards of the Neighborhood Bus Shelters, she was told that they may be an opportunity for work on a panel that is going to be installed, possibly silhouettes with lighting behind. Jenny will have to follow up with JR for a rendering and bring it back to the next meeting for discussion with the PAAB.

Jenny continued with cross walk art that has been talked about Pre-Pandemic. There are some proposed locations for crosswalk art, those locations include Miner's Hospital, Heber and Main, 5th Street and Swede, and 4th Street and Swede. City Engineer advised that Swede and Heber is not a good area to place crosswalk art. The purpose of this project is bring light to pedestrianized areas and bring awareness to these areas to high pedestrian usage.

Jenny advised for the board to pick one, maybe two locations to start, this project costs 10,000 for each location. The material used for this project is costly, including wear and tear. Jenny provided examples and research in the packet for the PAAB to review and look at until further discussion.

(1) Board Questions /Comments and Discussion

Pam asked if the Miners Hospital location should be moved down the list due to the bike lanes and traffic areas. Jenny replied that any work done around the Miners Hospital will impact the project, the City Engineer showed (the loop) around the Miner's Hospital that will not have impact on traffic. Jenny added that a fun idea to collaborate with the summer camp and have kids be involved. Just an idea if the PAAB is interested.

Jennifer Gardner asked if crosswalk art be used near the horse statue. Jenny recommended to not have any crosswalk art done on Park Ave, due to high traffic areas.

Tana Toly added that she attended a meeting with engineers, and it was stated that in 2024 there will be construction done on Park Ave, advised to save any projects slated for Park Ave.

Sam asked with new board members, when is the next time to do strategic planning? Jenny replied as of right now there are projects that are in the queue and wants to get back to Council with a PAAB update, possibly January/February council meeting. Jenny added that a strategic planning can be held in January or February if the board would like.

Jenny went over the projects that the PAAB is working on, Utility Boxes, Neighborhood Projects which was a number one priority project, Daly West Sculpture on Rail Trail, Elk Herd donation, and Bus Shelters that Jenny needs to follow up on.

Tana Toly added that Council has a focus on safety and to incorporate the art with the safety. Giving any ideas to help PAAB with safety and art.

Lara added to move forward with priority projects; utility boxes, Daly West Sculptures, and intersection art. Jenny would like to bring back intersection art back in January to get locations finalized.

Pam asked if the area around Miners Hospital location can fold into neighborhood art? Pam has a concern that crosswalk art that has already been placed with parks dept.

Pam states that bus shelters would have greater visibility and opportunity, Jenny replied that she will need more clarity with City Engineer on the Bus Shelters. Jenny states that bus shelters can be done next year, there's opportunity with them. Jenny will follow up with the intersection art and bring it back in January with more clarity from JR.

Pam asked about the post card mailer. Jenny replied that they spoke with the Community Engagement team and asked to work on design to bring back to the PAAB. Jenny will follow up on mailers and secondary homeowners. Once the design of the post card is finished, Community Engagement team will come back to a future meeting to discuss and answer questions the PAAB have.

(2) Open for Public Comment no comment

(3) Board final discussion and summary/direction.

Jenny recapped, the PAABs priority projects, utility boxes, Daly West Sculpture, intersection art and bus shelter. In the last meeting the board wanted the neighborhood art.

Next meeting to work on strategic planning and voting on projects.

Meeting is adjourned at 6:26 p.m.

STRATEGIC
PLAN ([LINK](#))



DRAFT Proposal for Neighborhood Art Program from Public Art Advisory Board (PAAB)

Background/ Research:

With the City Council and the community elevating Housing and Social Equity to a Critical Priority, the PAAB is looking for ways to help connect neighborhoods and create community gathering within residential areas. After researching neighborhood art programs in various communities across the United States, the PAAB feels that this opportunity will help educate and engage the public in this conversation.

Research/Ideas include:

1. Boulder, Colorado: <https://boulderarts.org/public-art/murals/creative-neighborhoods-mural-program/>
2. Denver, Colorado: <https://www.broadwayworld.com/denver/article/Denver-Arts-Venues-Makes-Funding-Available-For-Creative-Neighborhood-Projects-20190910>
3. Neighborhood Postcard Project: <http://www.creativeinterventionists.com/neighborhood-postcard-project>
4. Flagstaff, Arizona: https://www.flagstaff.az.gov/DocumentCenter/View/47296/Neighborhood-Grants-Application-081715-Fillable_distributed?bidId=
5. Washington DC: <https://www.cherryblossomworkshop.com/>
6. Creative Community Ideas: <http://www.greatneighborhoods.org/wp-content/uploads/2018/05/CCG-Summaries-Round-1-5-X.pdf>
7. Washington, DC <https://www.victorydancecreative.com/dc-neighborhood-project>

Proposed Budget:

PAAB would allocate up to \$2,500? per project (matching funds) x 5 neighborhoods (total is \$12,500 annually). Such funding must be spent on the artist time, art materials and/or infrastructure for the artwork. This is possibly more like a grant program, less like a RFP/project base program.

Other Possible Partners/ City Departments

Arts Council, Park City Foundation, Mountain Lands Community Housing Trust
Housing Dept (Jason Glidden), Planning Dept (LMC requirements), Budget, Finance and Legal

Timeline:

Goal is to establish a specific proposal by March 2020, with questions for Council to consider and further timeline on implementing project (spring/summer 2022?) Staffing should be addressed with this project. Initially, having applications available in the fall including City list of approved artists, with selection of neighborhood projects in the winter and implementation in Spring summer 2022seems feasible.

Marketing & Public Relations:

Park City would take the lead on any public relations regarding this proposed project. As projects are worked on, depending on partners and timeline, other partners or participants may also help with Marketing and Public Relations.

Summary:

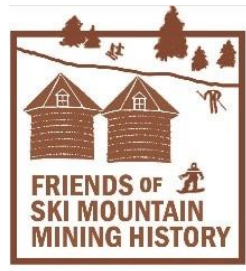
Park City is looking to partner with neighborhoods to bring art to the community. The goal of this project would be to bring neighborhoods together, create a sense of place, through art and beautify residential areas around Park City. For phase 1 of this project, the City will choose 5 neighborhoods to pilot this program and match up to \$2,500 in matching funds through grant program. Park City would engage with the public in the following ways.

1. This would allow the City to help fund neighborhood projects, but the City would not necessarily be the project manager of the artwork.
2. Eligible neighborhoods/HOAs would include those in 84060 City Limits.

3. City provide an 'Approved Artist List' for neighborhoods to choose from – Artists are from the region, must be in driving distance.
4. Artwork can be (what mediums?) Murals only? 2D only? Sculptural?
5. Artwork can be on public property or on private property? – This may change role of City.
 - Neighborhood Parks?
 - Fences, Garages or Homes that must be visible from the public right of way?,
 - Bus Shelters in neighborhoods?
 - Utility Boxes?
 - If artwork is not visible, what about public display (like on walls in City building or in an art gallery or partner business?)
 - What do we want to focus on?
6. The artwork/project must be lasting/ permanent (capital funds). Need to do further research if temporary in nature.
7. The artwork must be visible from the public right of way (cannot be private/interior) and neighborhood must select from one of the artists on the Approved Artist list.
8. Maintenance of the artwork is the responsibility of the HOA/ entity that does the project. If on City Property, City is responsible for maintenance?
9. Do we want preference to be given to any or all of the following?
 - Affordable Housing Areas
 - Latino/ Underserved Populations?
 - Tie to Sustainability, Transportation and Social Equity?
 - Tie to City's Core Values?
 - Other ideas?

Presentation to: Park City Municipal Corporation – 12/29/22

DALY-WEST PUBLIC SCULPTURE PROPOSAL



Sandy Brumley – Business Manager
sandy_brumley@yahoo.com
203-832-5156

Carol Spiegel – Art Design Manager
theartgroup@aol.com
702-371-6269



A BRIEF HISTORY

- The Daly-West Mine, located near the Montage Deer Valley and Empire Canyon Lodge, was founded in the 1880s.
- The mining buildings burned almost to the ground in 1913, including the wooden headframe.
- A new building was constructed during 1914 which included a large steel headframe used to raise and lower 2 different elevator platforms in the mine shaft.
- On Easter 1974, the surrounding building again burned, leaving the steel headframe as a remnant.
- In May of 2015, a cave-in around the mine shaft caused the headframe to collapse into the cavity, and it was moved to its current location so the shaft could be capped.



A NEW DAY FOR THE DALY-WEST HEADFRAME

- Deer Valley Resort purchased the land surrounding the headframe in 2020.
- In fulfillment of a 2020 Agreement between Empire Pass Master Owners Association and Park City Municipal Corporation, the two organizations funded the project to preserve this relic of the town's mining history
- The Headframe was:
 - Repaired to stand again on its own
 - Moved 100 feet up the hill to more stable ground



Photo showing proposed headframe location relative to Montage and original location



Photo showing proposed headframe location as seen from Empire Express lift queue

AN OPPORTUNITY TO EXPAND UPON THE LEGACY

- Deer Valley, which acquired ownership of the steel structure as part of the preservation project, has donated the mangled steel remnants to The Friends of Ski Mountain Mining History
- The Friends offer this proposal to the Park City Municipal Corporation to create a memorial to the:
 - Successful preservation of the headframe itself on 7/1/22
 - Estimated 34 miners who died in the 1902 Daly-West Mine explosion
- The Friends seek to:
 - Donate a subset of the remnants to the city
 - For the creation of a large public sculpture by a local artist
 - That would be placed at the head of The Rail Trail



POTENTIAL LOCATION FOR ART PROJECT

Potential Location for Daly West Headframe Art Project



ARTISTIC VISION

- In their current form, the twisted remnants speak to the power and tragedy of the 2015 collapse of the headframe
- And provide an interesting platform for a local artist to:
 - Create a dramatic sculpture
 - Inspire Park City residents
 - Connect the skiing, mining history and artistic communities
- We ask the city to:
 - Issue a request for proposal to local artists with a \$60,000 budget
 - To submit their ideas for how to create a sculpture for prominent display
 - Including designs to be rendered as drawings
 - And a specific budget for the fabrication and installation of the artwork
- The Friends will:
 - Collaborate with:
 - The PAAB
 - The Arts Council of Park City Summit County
 - Participate as active members of the design and review team
 - Provide historic background and color to inspire the artists



FOSMMH BUSINESS PROPOSAL

- This proposal is from The Friends of Ski Mountain Mining History:
 - A working committee of The Park City Museum
 - Mailing address:
 - FOSMMH
 - PO Box 555
 - Park City, UT 84060
 - Estimated value of the steel to be donated to PCMC:
 - An educated guess on the weight of steel is 4 to 5 tons
 - Scrap value of structural steel is \$70 per ton
 - The total value of the steel is \$280 to \$350
 - If we use 33 to 50% of the steel for the city sculpture, the donated value to the city will be between \$93 and \$175
 - FOSMMH asks that the city include the following requirements in its Request for Proposal to Artists:
 - Proposers should utilize all of the 76 feet of undifferentiated mangled steel in the attached inventory
 - To create 2 or 3 sculptures based on the esthetic recommendation of the sculptor
 - The goal is to use all of the steel in esthetically pleasing sculptures, and to avoid leaving FOSMMH with remnants that cannot be used in a second or third sculpture
 - The city will chose which if the designs it prefers and fund that initiative
 - FOSMMH are in discussion with others about funding additional artworks in other locations



NEXT STEPS

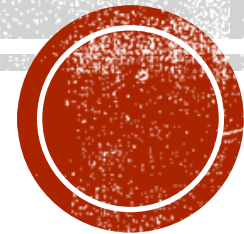
- The Public Arts Advisory Board will present this proposal to Park City Council on 1/12/23.



Daly-West Mine

MANGLED GOODNESS

Inventory of Major Sections



sandy_brumley@yahoo.com

203-832-5156

OVERVIEW

Major sections consist of:

- 19 foot highly mangled z-riveted section
- 18 foot highly mangled straight plate section
- 12 foot z-riveted section
- 8 foot straight plates section
- 7 foot 6 in z-rivet section
- 6 foot z-riveted section
- 8+5+11 foot highly mangled section

For a total of 76 undifferentiated liner feet

We also have:

- 2 boxes of miscellaneous smaller pieces
- 10 to 12 small miscellaneous pieces



19 FOOT Z-RIVETED HIGHLY MANGLED SECTION



**18 FOOT
HIGHLY MANGLED
STRAIGHT PLATES**



12 FOOT Z-RIVETED STRAIGHT SECTION



8 FOOT STRAIGHT PLATES SECTION



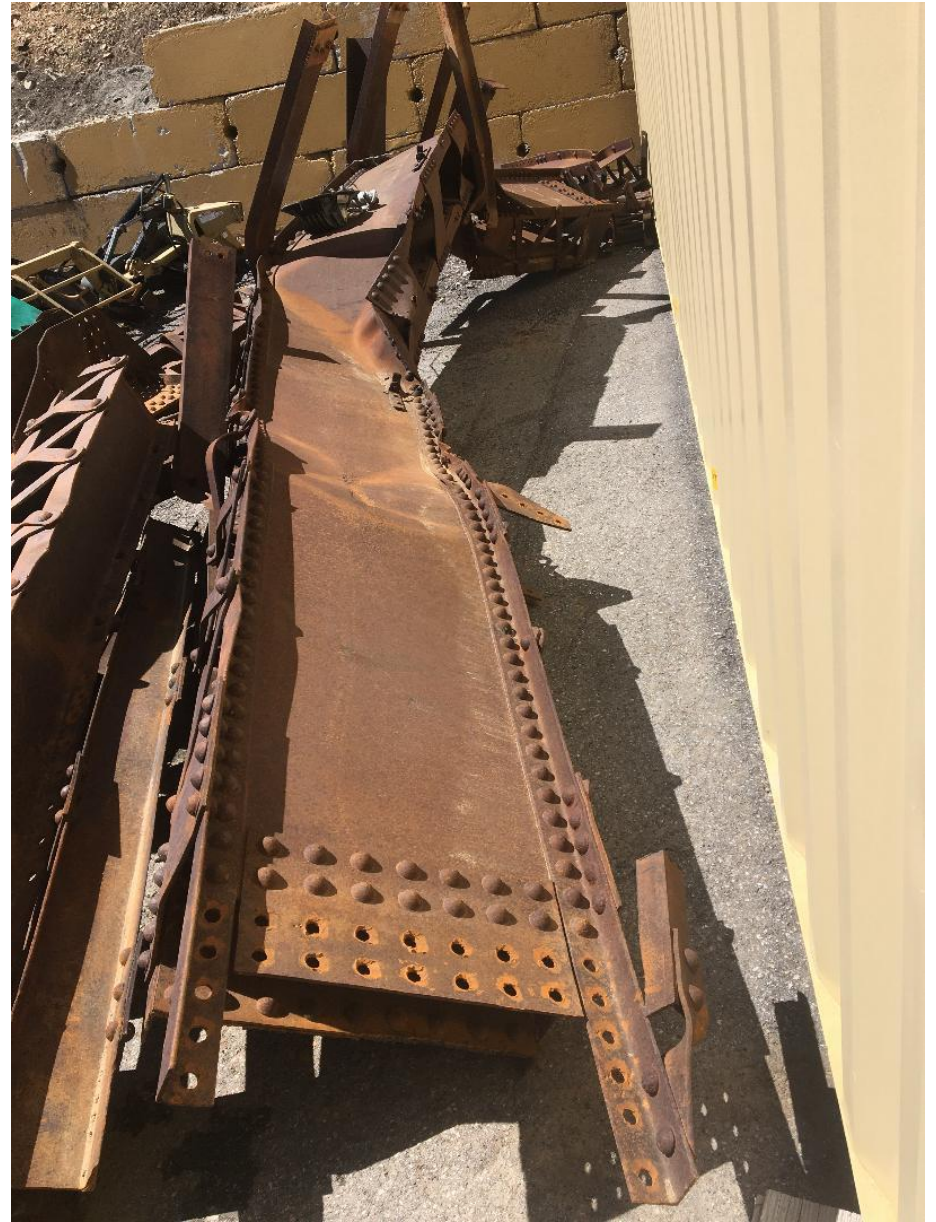
6 FOOT Z-RIVETED SECTION



**7 FOOT Z-RIVETED
SLIGHTLY MANGLED
SECTION**



**8 + 5 + 11 FOOT
HIGHLY MANGLED SECTION**



BOX 1 SMALL PIECES



BOX 2 SMALL PIECES



**10 TO 12 MISCELLANEOUS
SMALL PIECES**



November 29, 2022

Save People Save Wildlife (SPSW) proposes to donate two reflective elk silhouettes to Park City's Public Art Collection. The pieces would be called "Our Elk Herd." The elk silhouettes would not only be an attractive addition to the city's art collection but would also provide a message: "art for awareness."

Save People Save Wildlife is a registered 501 (c) 3, non-profit, which was started in 2015. Full information can be found on the web at www.savepeoplesavewildlife.org.

Save People Save Wildlife has as our mission reducing or eliminating vehicle/wildlife collisions.

Success in our area include the wildlife overpass at Parley's Summit and associated wildlife fencing, escape ramps, and cattle guards along I-80. This has been accomplished through our working relationship with UDOT and financial support from the community to cost-share wildlife mitigation measures with UDOT (\$168,000 to date).

SPSW also successfully worked with Summit County to have wildlife mitigation measures included in the recently passed \$50 million General Obligation Bond for open space and to have wildlife mitigation measures considered on critical projects in the county's Long Range Transportation Plan.

SPSW and UDOT have been working together for a while to address the serious vehicle wildlife collisions on SR224. Three decades of vehicle wildlife crash data plus recent UDOT research reports have identified SR224 as the #1 hotspot in UDOT Region 2 for vehicle wildlife collisions. SR224 has the fifth highest number of vehicle wildlife crashes per mile per year of any highway in Utah. The worst location along SR 224 is at mile marker 7.5, near the McPolin Farm. The local elk herd in the area crosses SR 224 hundreds of times a year.

Until permanent wildlife mitigation measures can be implemented, the only way to reduce vehicle wildlife collisions is to affect and change driver behavior. SPSW proposes the addition of elk art to Park City's art collection will raise awareness of elk herd crossing SR 224 and alert drivers of their responsibility to avoid vehicle collisions. "Art for Awareness" will help.

The proposed donation is two reflective elk silhouettes. Each elk silhouette is 9 feet wide, 9 feet tall, and 2.25 inches thick. The material used is mild steel for the skeleton and mild steel for the silhouette. They have been designed to show the outline shape of the elk. They are coated with Rustoleum primer and 2 coats of Rustoleum flat black. The outline is covered with 3M 3430 engineering grade reflective sheeting and 3M 3/16- inch aluminum tape on edges. Base steel is 1.5-inch square tube with 0.120- inch wall coated with primer and flat black. Base bolts are grade 5.5inch coarse threaded. Minimal maintenance may be required, such as touch up painting and reapplying reflective sheeting, which has an expected life of 7 years. Washing with a mild soap and water every few years may be useful.

They will be highly visible at night from oncoming traffic. Each silhouette is valued at \$3,500.00 (\$7,000 total). Their proposed location is roughly between the Farm Trailhead parking lot and the old McPolin Farm cattle shed area.

The artist is Bland Hoke. He is an exciting young designer on a global mission to inspire. Combining a unique background in public art with a love of the outdoors and cutting-edge urban design, Bland collaborates with other creatives and stakeholders to bring forth fresh solutions and unexpected possibilities. Bland is the principal at Bland Design LLC and former Staff Artist at Jackson Hole Public Art. His process is a direct response to places and people, marked by a true understanding of contemporary design and a signature resourcefulness, courtesy of his Western roots.

Bland has received an Innovation by Design award from Fast Company, presented at Design INDABA in Cape Town, South Africa and recently fabricated a 40' inflatable Elk Antler. He thrives on building thoughtful public places, developing temporary artwork and imagining projects that feature hammocks.





DRAFT Proposal for Artistic Intersection Program with Public Art Advisory Board (PAAB)

Summary:

Pedestrian Crossings are an important part of the Park City Community, walkability and overall Transportation Plan. City has identified 9 intersections (5 to be selected) on Park Avenue and Main Street to consider as part of an initial pilot program (See Attachment Location at end of this document). The goal of this project is to promote walkability, pedestrian safety, connect with the community, create a sense of place through art and beautify residential and commercial areas around Park City. If the project is successful, additional intersections may be implemented in future years.

Project Team & Partners:

Project Lead - Public Art Staff & PAAB

Project Team (City Departments) - Streets, Engineering, Transportation Planning, Walkability, Economic Development, Housing, Community Engagement, Planning, Budget, Finance and Legal

Community Partners - Arts Council, Park City Foundation, Kimball Art Center, HPCA, Park City School District

Timeline:

April - Release Call for Artwork & RFP for Service Provider to install artwork.

May – PAAB & Staff recommend artwork and Service Provider to Council at May Meeting

June - Finalize Contracts, and produce project in June.

Maintenance: Artwork will fade overtime with street maintenance (winter conditions – salt and plowing). Intersections will need to be maintained annually each spring/early summer. Each year, the City can decide if they want to put out a new call for artwork, reinstall an existing artwork or discontinue the program. Ongoing funding is needed to implement additional intersections or maintain current ones.

Proposed Budget: \$42,000

\$400 for 5 artist = \$2,000

\$40,000 for Service Provider to complete installation.

Funding is from the Public Art Advisory Board's budget – including Lower Park RDA and 1% of Main Street Improvements. *Changes in locations or artist process may change budget

Possible Project Themes (Choose One):

1. Between the Lines – Artwork represents various perspectives.
2. Breaking Boundaries – Artwork represents social equity themes.
3. Connected Colors – Artwork represents connectivity and colors in our community.
4. Walk This Way – Artwork represents natural environment, and/or Arts and Culture – things that represent Park City.

**Through a community theme that is chosen, the community will be encouraged to think of innovative ways to design intersections.

Community Involvement & Artist/Service Provider Selection Process:

The City will release a Call for Art to for greater Summit County community to participate in. Students, Residents, Businesses or Employees in Summit County may submit designs. No logos or branding may be used. No profanity or nudity will be allowed. The City will select up to 5 designs where the artist/artist team will be rewarded \$400 and the original artwork placed into the City's Public Art Collection. Submit designs that are either geometric, tessellations or highly stylistic. Artist should submit no more than one page summary of how their design fits the theme. Total Budget for Artist is estimated at \$2,000. This will come from PAAB's Lower Park RDA and Main Street Improvement Budget.

The City will release a RFP for a Service Provider to implement intersection designs. The Service Provider will be responsible for taking the 5 selected designs and under the supervision of City Staff installing the designs in the intersections using materials. Project Maximum is \$40,000 to complete all 5 intersections. Must guarantee material for at least a year. Materials may include reflective paint, 3M or themos. Total Budget is \$40,000. This will come from PAAB Lower Park RDA and Main Street Improvement Budget.

Background/ Research:

Staff has researched Intersection art programs in various communities across the United States. The PAAB feels that this opportunity will help connect to the community with the community critical priority of transportation.

Research/Ideas include:

MidTown Atlanta: <https://www.ajc.com/opinion/opinion-how-public-art-improves-street-safety/6E6EABQVHNFI7DYMJHJFDP3G2Q/>

<https://www.goodshomedesign.com/colorful-road-art-prevents-traffic-accidents/>

Marketing & Public Relations:

Park City would take the lead on any public relations regarding this proposed project. Arts Council will help promote call for art, opportunities to participate and story during/after project completion. Other partners or participants may also help with Marketing and Public Relations as the City sees fit.

Location Map: (Potential Locations)

1. Miners Hospital
2. Heber and Main
3. Swede and Heber
4. 5th Street and Swede
5. 4th Street and Swede

RESOLUTION NO. 2021-1

A RESOLUTION GOVERNING THE USE OF ELECTRONIC MEETINGS BY THE PARK CITY PUBLIC ART ADVISORY BOARD

WHEREAS, Park City values transparency and conducting the public's business in public meetings where actions are taken and deliberations are conducted openly;

WHEREAS, while the preference is for Park City public bodies to convene in person, it is understood that there may be circumstances where electronic meetings are necessary;

WHEREAS, the Open and Public Meetings Act was amended during the 2021 General Session to grant additional flexibility in how public bodies conduct electronic meetings;

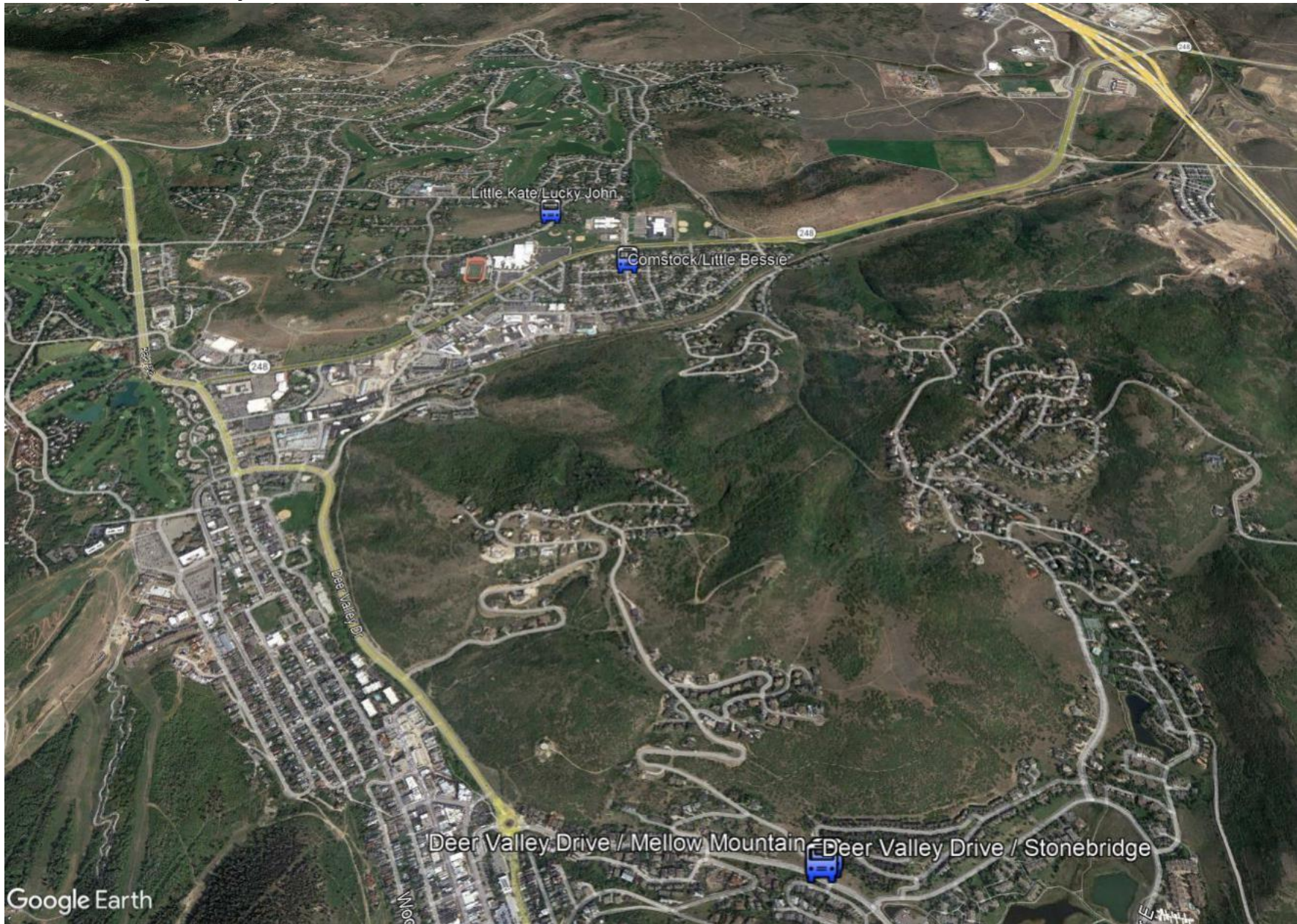
WHEREAS, the COVID-19 virus continues to raise health or safety concerns for in-person public meetings and electronic meetings help facilitate social distancing;

BE IT ORDAINED BY PARK CITY PUBLIC ART ADVISORY BOARD OF PARK CITY, UTAH, THAT:

The Park City Public Art Advisory Board may hold electronic meetings in accordance with the Park City Public Art Policies and section 52-4-207 of the Utah Code, as amended.

PASSED AND ADOPTED this 13th day of December, 2021.

Transit Stop Art Options



Potential Locations: Comstock/Little Bessie, Deer Valley Drive / Mellow Mountain, Deer Valley Drive / Stonebridge, Any of the stops along Deer Valley Drive North, and Corner of Little Kate/Lucky John

BUS STOP ADA COMPLIANCE

JUNE 2021

CONCEPT REPORT



2001 PARK AVE.



- 1 ADD CONCRETE BUS PULL OUT AREA TO REDUCE TRAFFIC CONFLICTS ON SR-224.
- 2 LOWER EXISTING MANHOLE
- 3 RELOCATE FIRE HYDRANT
- 4 EXISTING VMS BOARD
- 5 PROPOSED BUS SHELTER, BENCH, AND CONCRETE PAD
- 6 EXISTING SIGN TO REMAIN
- 7 RELOCATE BUS STOP SIGN

CRITICAL ELEMENTS:

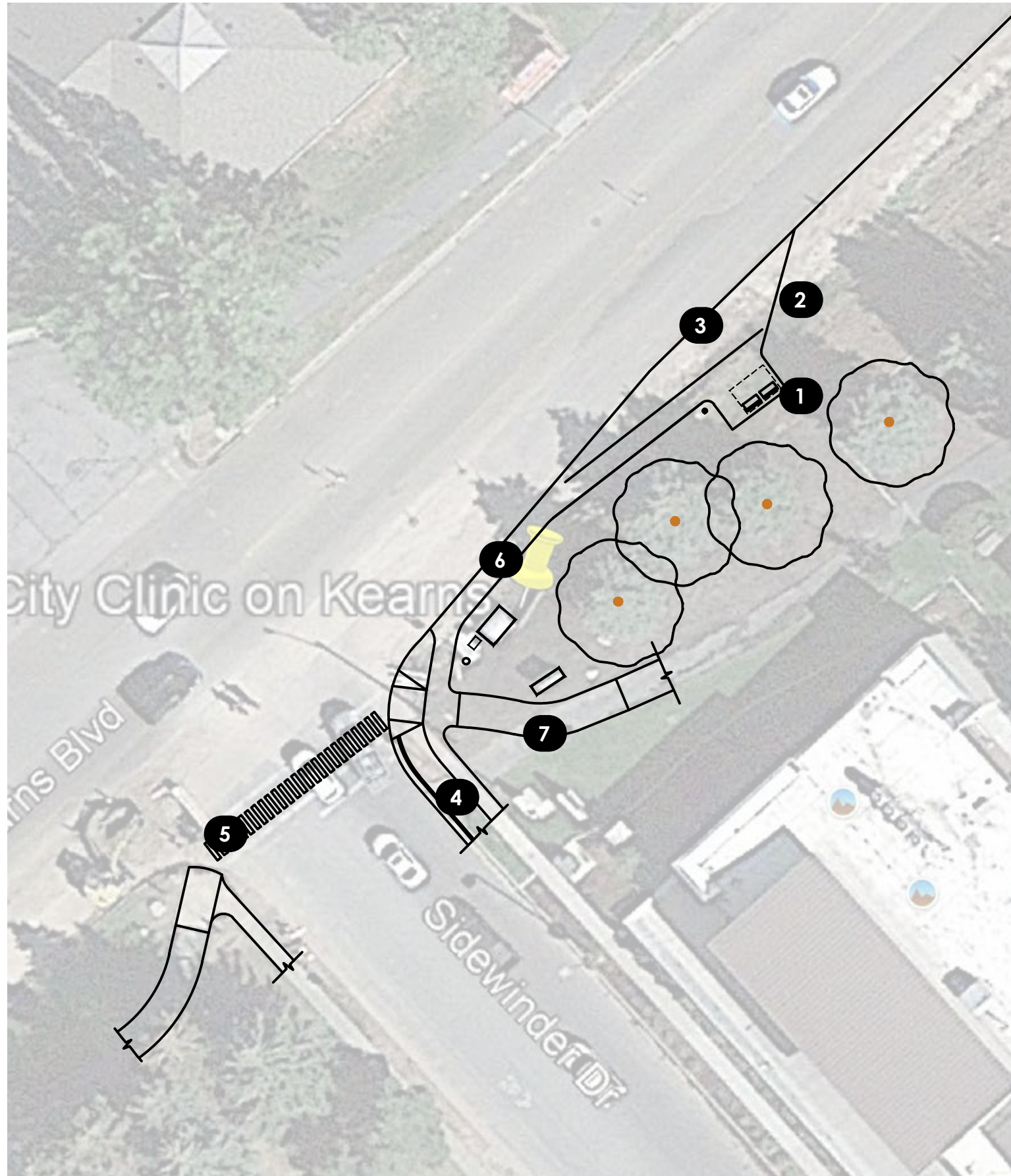
- COORDINATION WITH UDOT FOR BUS PULL OUT AREA
- RELOCATION OF FIRE HYDRANT
- EXISTING MANHOLE WILL LIKELY NEED TO BE LOWERED.
- RETAINING LIKELY REQUIRED/ IMPACTS TO HOTEL PARK CITY LANDSCAPE

Stop Location: 2001 Park Ave.

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	Yes	Dimension and Slope compliant, surface does not
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	Dimension and Slope compliant, surface does not
	Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	Dimension and Slope compliant, surface does not
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No Shelter
		Connected by an accessible route to boarding and alighting area	No	No route or shelter
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	Partial	Existing sidewalk meets slope and width requirements but does not connect to loading area.
		Accessible route to streets, sidewalks, and pedestrian paths	Partial	Existing sidewalk meets slope and width requirements but does not connect to loading area.
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Partial	Existing sidewalk meets slope and width requirements but does not connect to loading area.
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Partial	Existing sidewalk meets slope and width requirements but does not connect to loading area.
	Width	$\geq 36''$ (exception: may be reduced to 32'' minimum for a length of 24'' provided reduced width segments are separated by segments that are a minimum of 48'' long by 35'' wide)	Partial	Existing sidewalk meets slope and width requirements but does not connect to loading area.
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No Bench

Other Comments: Roadway shoulder under construction, was unable to access for measurements. Need pull out area to reduce traffic conflicts on SR-224

PARK CITY CLINIC ON KEARNS



- 1 PROPOSED BUS SHELTER WITH CONCRETE PAD AND BENCHES
- 2 PIPE EXISTING OPEN DITCH
- 3 CONCRETE PULL OUT FOR BUS STOP
- 4 CLOSE OFF EXISTING RAMP AND RE-ALIGN TRAIL TO INTERSECTION
- 5 RESTRIPE CROSS WALK
- 6 NEW 5' CONCRETE SIDEWALK
- 7 REROUTE ASPHALT TRAIL

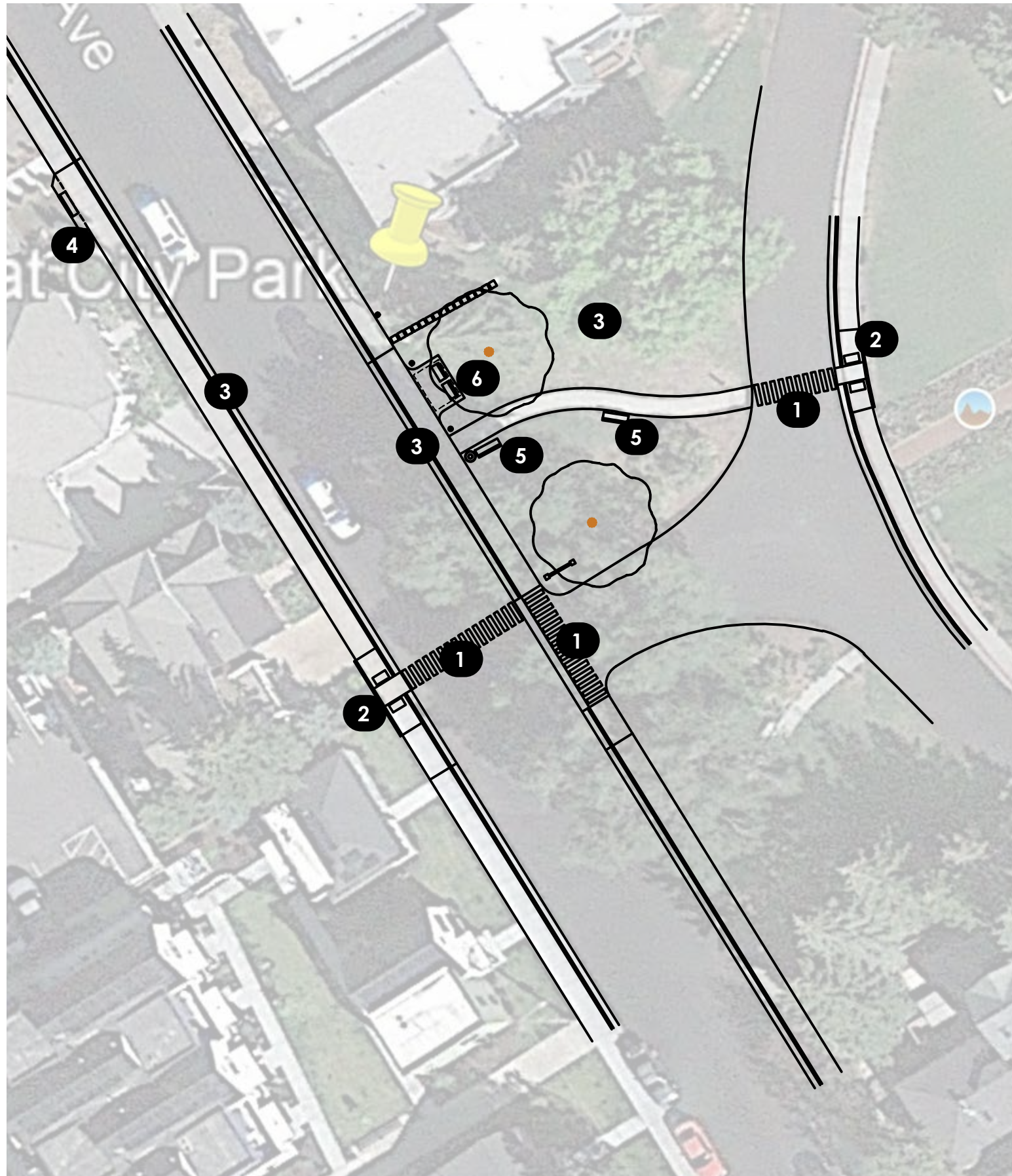
- CRITICAL ELEMENTS:**
- COORDINATION WITH UDOT FOR BUS PULL OUT AREA
 - PIPING OF EXISTING OPEN DITCH

Stop Location: Park City Clinic On Kearns

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	No	None
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	No	None
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	No	Roadway shoulder is approximately 8-12% for 14' from EOA. Landscape berm starts at 14' from EOA and is 14%. 12' from EOA to Utility box.
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	No	
Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	None, Roadbase from roadway shoulder.	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	None
		Connected by an accessible route to boarding and alighting area	No	None
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	
		Accessible route to streets, sidewalks, and pedestrian paths	No	Sidewalk to clinic is in good condition and meets ADA slopes. Trail has ped ramp separate from sidewalk. Does not align with intersection. Existing Ped Ramps are 4-6%. Crosswalk needs to be repainted.
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	No	None
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	No	None
Width	$\geq 36''$ (exception: may be reduced to 32'' minimum for a length of 24'' provided reduced width segments are separated by segments that are a minimum of 48'' long by 35'' wide)	No	None	
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	None

Other Comments: Storm Drain outfall (12-18" pipe) daylights within area that will likely be needed for stop. Will require piping open ditch. Unknown origin of pipe but appears to come from the Northwest under Kearns Blvd.

1378 PARK AVE AT CITY PARK



- 1 ADD STRIPED CROSS WALK
- 2 NEW ADA RAMP
- 3 REPAIR/REPLACE CURB AND GUTTER/ SIDEWALK
- 4 WIDEN EXISTING SIDEWALK TO MEET BUS STOP ADA REQUIREMENTS
- 5 EXISTING BENCH
- 6 NEW PAD, BUS STOP SHELTER AND BENCH

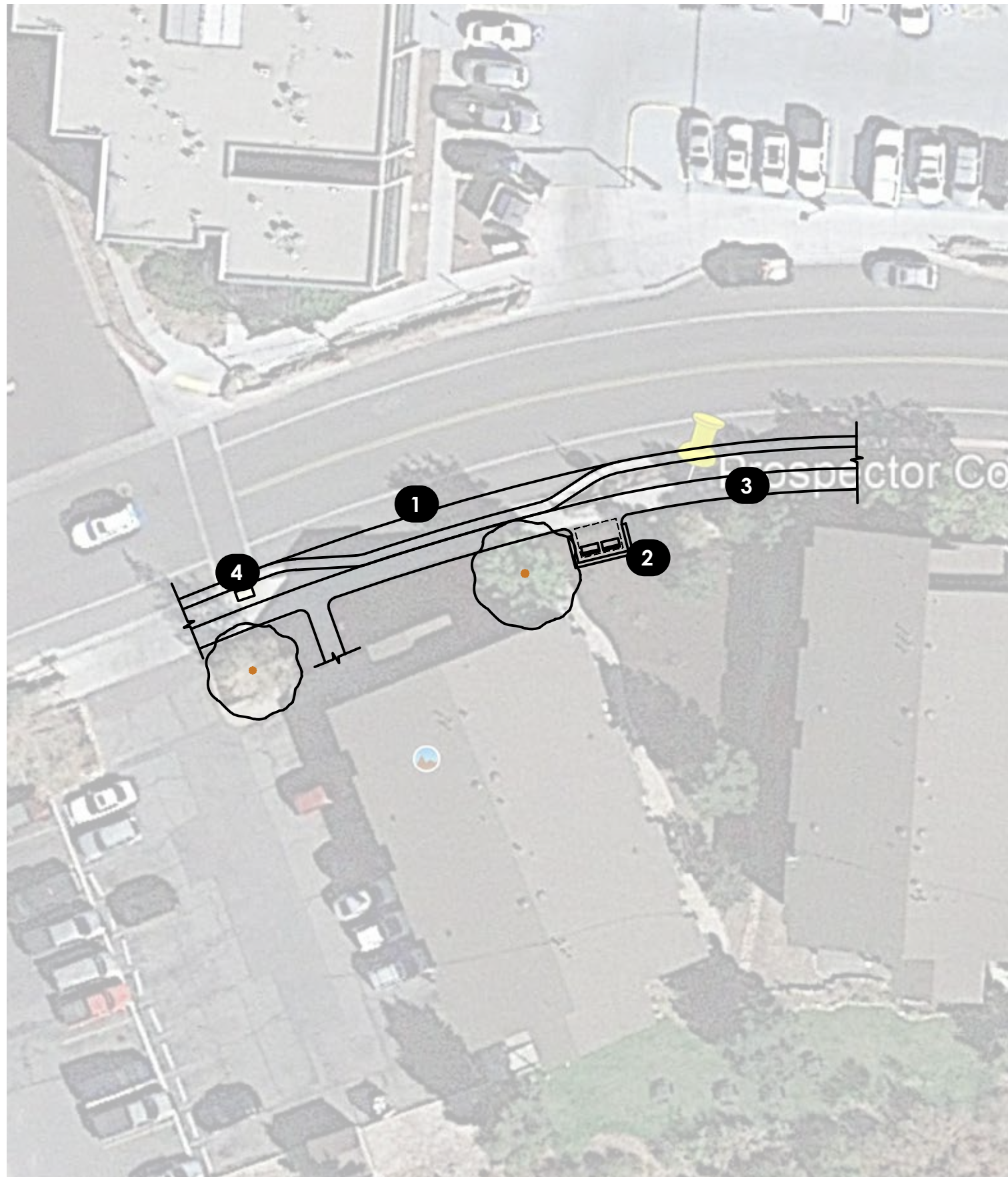
- CRITICAL ELEMENTS:**
- STOP ON WEST SIDE OF PARK AVE. IS EXTREMELY SPACE LIMITED.
 - LAND OWNERSHIP FOR WEST STOP.

Stop Location: 1378 Park Ave. @ City Park

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Existing Curb Back Sidewalk
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	No	Space available with manageable grade
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Existing slopes are adequate for improvements, no major grading or retaining needed.
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	Existing slopes are adequate for improvements, no major grading or retaining needed.
Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	Partial	Need to expand	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	
		Connected by an accessible route to boarding and alighting area	No	
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No crosswalk across street
		Accessible route to streets, sidewalks, and pedestrian paths	No	No crosswalk across street
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Yes	Curb back sidewalk
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Yes	Curb back sidewalk
Width	$\geq 36''$ (exception: may be reduced to 32'' minimum for a length of 24'' provided reduced width segments are separated by segments that are a minimum of 48'' long by 35'' wide)	Yes	Curb back sidewalk	
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	Existing bench is at an adjacent walkway

Other Comments: Plenty of open space for further development of the existing bus stop.

PROSPECTOR CONDOS ON
PROSPECTOR AVENUE



- 1 EXISTING PULL OUT AREA
- 2 PROPOSED BUS SHELTER, PAD, BENCH AND RETAINING WALL
- 3 EXISTING SIDEWALK
- 4 EXISTING ADA RAMP AND DETECTABLE WARNING

CRITICAL ELEMENTS:

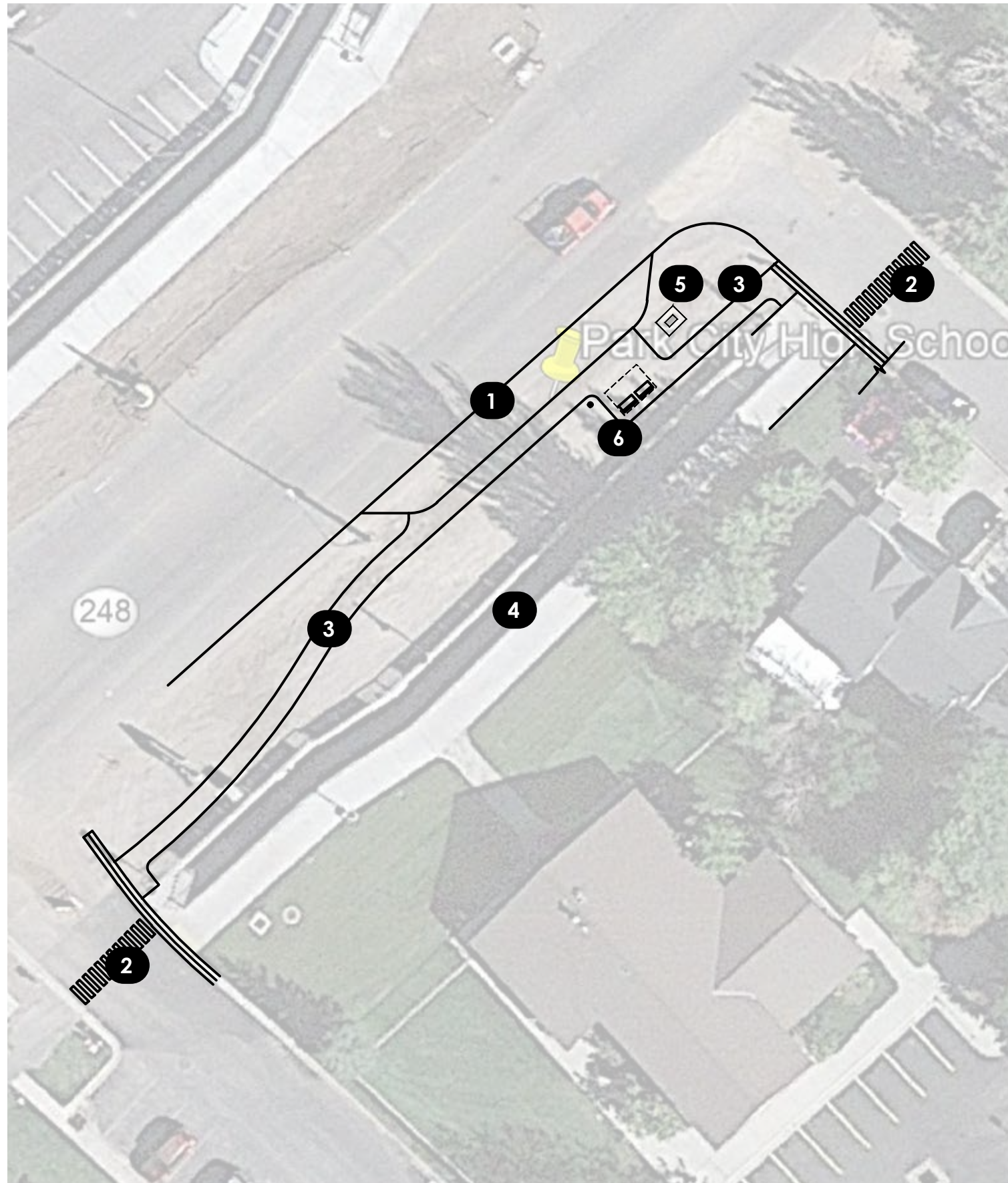
- RETAINING AND IMPACTS TO EXISTING LANDSCAPE IN FRONT OF CONDOS.

Stop Location: Prospector Condos on Prospector Ave.

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Existing Sidewalk
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	No	Sidewalk only, no additional space
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	No	Will need to retain adjacent berm
Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	Partial	Sidewalk complies, need to expand	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No shelter
		Connected by an accessible route to boarding and alighting area	No	No shelter
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	Yes	
		Accessible route to streets, sidewalks, and pedestrian paths	Yes	
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Yes	
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Yes	
Width	$\geq 36''$ (exception: may be reduced to 32" minimum for a length of 24" provided reduced width segments are separated by segments that are a minimum of 48" long by 35" wide)	Yes		
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No bench

Other Comments: Sidewalk, access, and crosswalks to other side of street are all compliant. No shelter, bench or waiting area. Sidewalk butts up against a landscape area for the condos with a berm and trees. A waiting area will require retaining and tree protection. Sidewalk grades are 1-2%

HIGH SCHOOL SOUTH OF KEARNS



- 1 PROPOSED CONCRETE BUS PULL OUT AREA
- 2 RESTRIPE CROSS WALK
- 3 PROPOSED SIDEWALK
- 4 EXISTING TRAIL SYSTEM/PEDESTRIAN TUNNEL
- 5 EXISTING STORM DRAIN INLET BOX
- 6 PROPOSED BUS SHELTER, PAD, BENCH, ETC .

CRITICAL ELEMENTS:

- DRAINAGE BETWEEN KEARNS AND TUNNEL.
- COORDIANTION WITH UDOT FOR BUS PULL OUT AREA.
- AVOID IMPACTS TO UTILITIES FROM TUNNEL PROJECT.

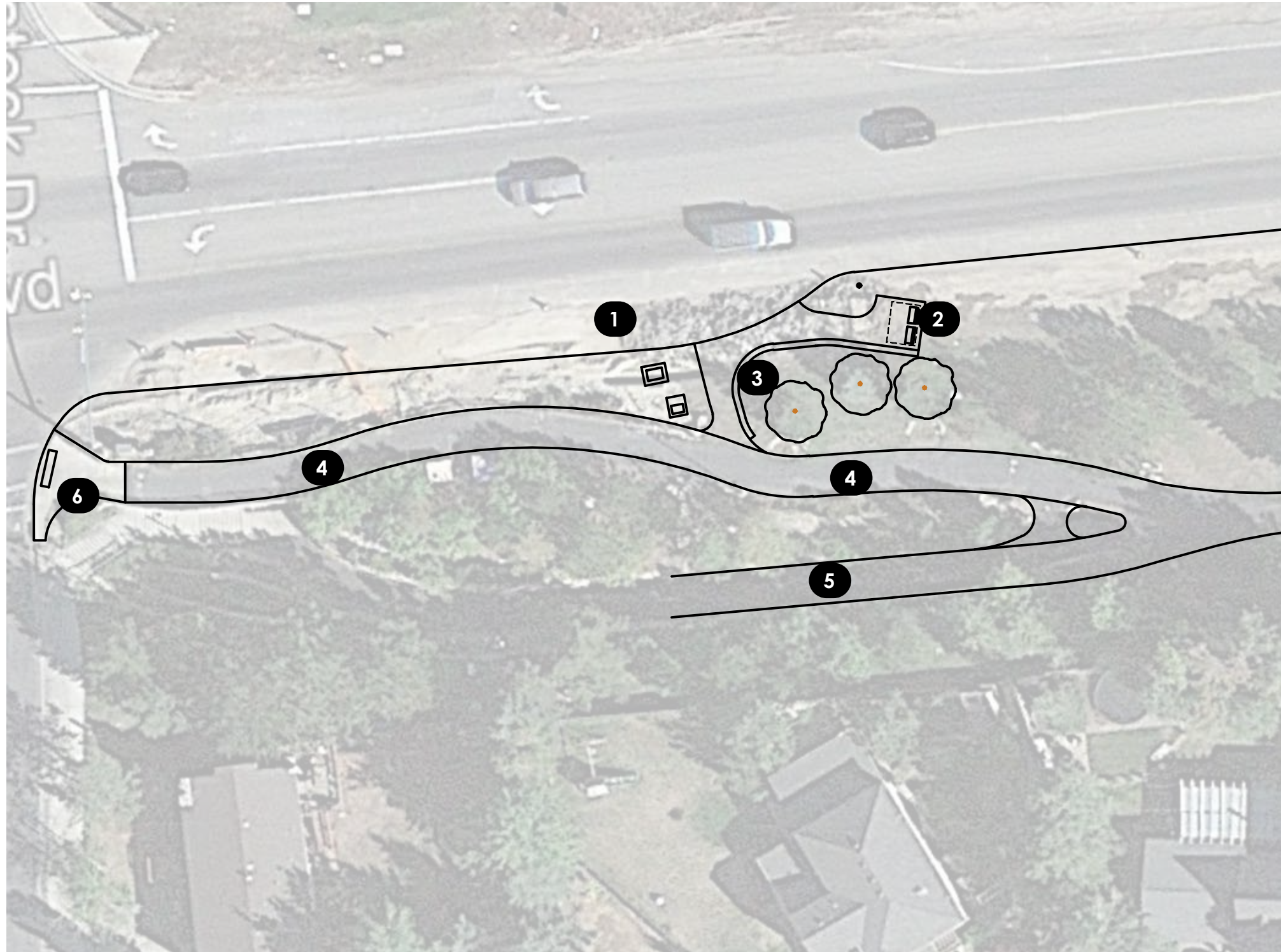
Stop Location: High School South Of Kearns Blvd.

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	Yes	Dimension and Slope compliant, surface does not
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	Dimension and Slope compliant, surface does not
	Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	Dimension and Slope compliant, surface does not
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No Shelter
		Connected by an accessible route to boarding and alighting area	No	No route or shelter
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No Existing Route
		Accessible route to streets, sidewalks, and pedestrian paths	No	No Existing Route
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	No	No Existing Route
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	No	No Existing Route
	Width	$\geq 36''$ (exception: may be reduced to $32''$ minimum for a length of $24''$ provided reduced width segments are separated by segments that are a minimum of $48''$ long by $35''$ wide)	No	No Existing Route
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No Bench

Other Comments: 27' from EOA to tunnel wall. 8' From EOA to sign. Needs Access from tunnel/crosswalk to stop. (provide access from east and west sides?) Crosswalk isn't striped. Check tunnel drawings for utility conflicts.

compliance form/field notes

COMSTOCK AND KEARNS



- 1 PROPOSED PULL OUT AREA
- 2 PROPOSED BUS SHELTER, PAD, BENCH AND RETAINING WALL
- 3 RETAINING WALL FOR EXISTING BERM/ SLOPE
- 4 EXISTING TRAIL
- 5 EXISTING RAMP FOR PEDESTRIAN TUNNEL
- 6 EXISTING ADA RAMP WITH DETECTABLE WARNING

CRITICAL ELEMENTS:

- AVOIDANCE OF EXISTING UTILITY BOXES,
- COORDINATION WITH UDOT ON PULL OUT AREA OFF KEARNS BLVD.
- RETAINING WALL TO ACCOMODATE BERM/EXISTING SLOPES.

Stop Location: Comstock and Kearns

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	Yes	Dimension and Slope compliant, surface does not
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Dimension and Slope compliant, surface does not
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	Dimension and Slope compliant, surface does not
Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	Dimension and Slope compliant, surface does not	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No Shelter
		Connected by an accessible route to boarding and alighting area	No	No route or shelter
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No Existing Route
		Accessible route to streets, sidewalks, and pedestrian paths	No	No Existing Route
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	No	No Existing Route
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	No	No Existing Route
Width	$\geq 36''$ (exception: may be reduced to $32''$ minimum for a length of $24''$ provided reduced width segments are separated by segments that are a minimum of $48''$ long by $35''$ wide)	No	No Existing Route	
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No Bench

Other Comments: Bus pull out area needed to reduce traffic conflicts on kearns.

HOLIDAY VILLAGE APARTMENTS



- 1 RESTRIPE EXISTING BIKE LANE
- 2 PROPOSED BUS SHELTER, PAD, BENCH
- 3 NEW BUS PULL OUT AREA
- 4 NEW 5' SIDEWALK
- 5 REPAIR/REPLACE EXISTING RETAINING WALL
- 6 NEW ADA RAMP WITH DETECTABLE WARNING

CRITICAL ELEMENTS:

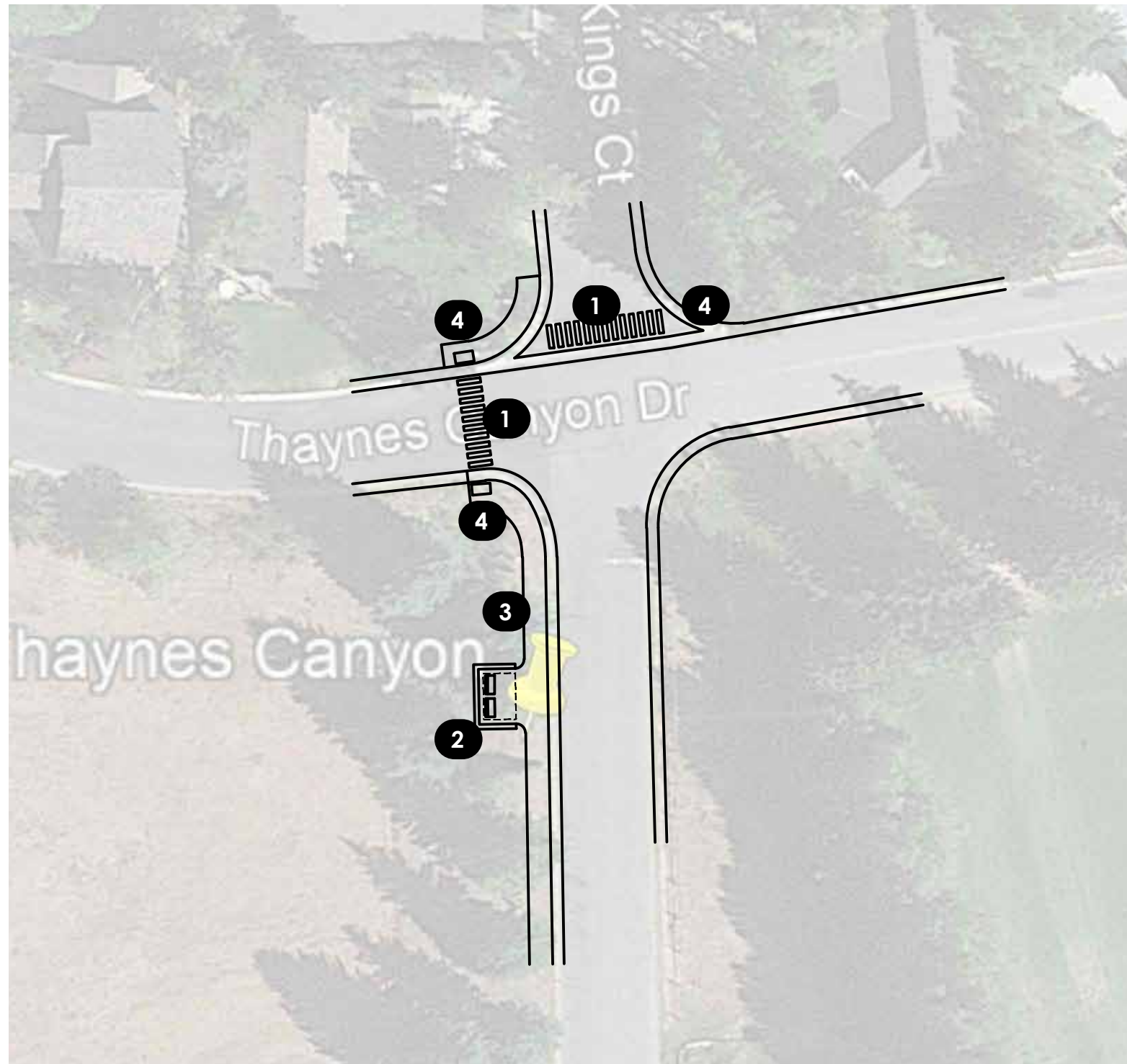
- MAJOR EARTHWORK AND REMOVAL OF MATURE TREES.
- EXISTING RETAINING WALL IS FAILING.
- REQUIRES SIGNIFICANT IMPACTS TO PRIVATE PROPERTY TO ADD ACCESSIBLE ROUTE TO APARTMENTS.

Stop Location: Holiday Village Apts.

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Existing Curb Back Sidewalk
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	No	Sidewalk is 5' wide and is pinned between curb and retaining wall
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Existing Sidewalk Slope is Compliant. Grade changes abruptly with retaining wall.
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	
Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	Yes	Existing Concrete	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No space for shelter. Recommend removal of trees and berm to create space for stop and shelter.
		Connected by an accessible route to boarding and alighting area	No	N/A
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No access to apartments
		Accessible route to streets, sidewalks, and pedestrian paths	Partial	Connected to sidewalk, not connected to anything else.
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Yes	
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Yes	
Width	$\geq 36''$ (exception: may be reduced to 32" minimum for a length of 24" provided reduced width segments are separated by segments that are a minimum of 48" long by 35" wide)	Yes		
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No Bench

Other Comments: No ADA access from apartments. Large berm east of stop, failing retaining wall against sidewalk. Berm is about 4-5' high at the highest point. Current wall is 18"-2' tall but slope grows behind wall and any additional space for a bench or shelter would require a larger retaining wall. How far do we chase the wall repair? All of it is failing? Do we provide ADA access back to apartments or is that the private property's responsibility? No sidewalk or access on west side of monitor drive, don't need cross walk. Does the crosswalk across the private drive need to be striped? Slopes on existing sidewalk are good (.25% cross slope, 2-3% directional slope).

THREE KINGS DRIVE AND TAHYNES CANYON ROAD



- 1 ADD CROSS WALK
- 2 PROPOSED BUS SHELTER, PAD, BENCH
- 3 NEW 5' SIDEWALK
- 4 ADD ADA RAMPS WITH TRUNCATED DOMES

CRITICAL ELEMENTS:

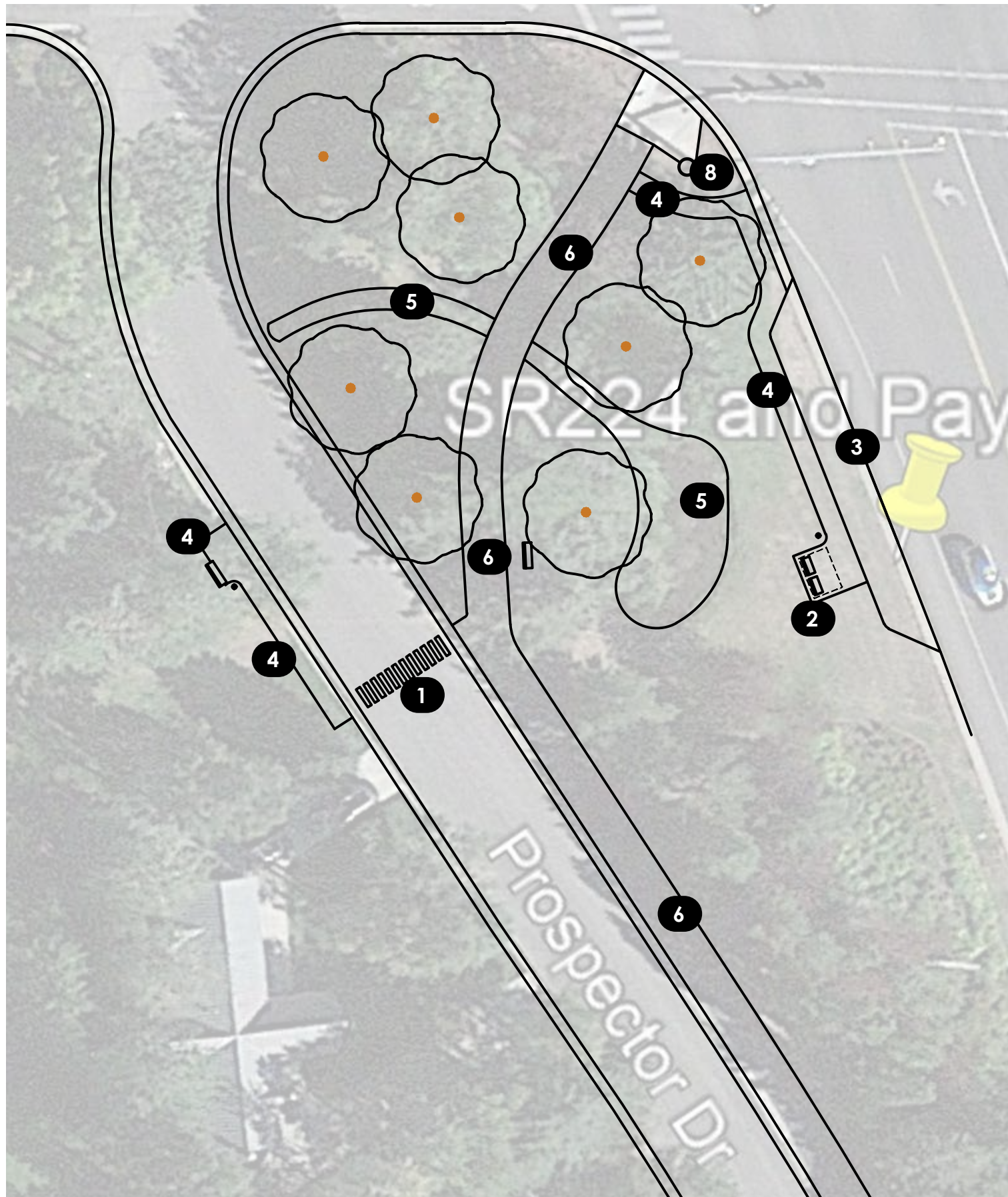
- STOP IS SPACE LIMITED WITH A MAJOR SLOPE AND EXISTING MATURE TREES ADJACENT TO SPACE.
- NEED TO DETERMINE FURTHER CONNECTIVITY/ACCESSIBILITY WITH SIDEWALKS IN AREA.
- 2-3 OTHER STOPS ALONG THREE KINGS DRIVE HAVE IDENTICAL ISSUES/LAYOUTS. THIS SITE COULD BE A MODEL FOR THOSE.

Stop Location: Three Kings Drive and Thaynes Canyon

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	No	Tight area between EOA and adjacent slope.
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	No	Tight area between EOA and adjacent slope.
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	No	Tight area between EOA and adjacent slope.
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	No	None
	Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	Existing Gravel
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	None, no space
		Connected by an accessible route to boarding and alighting area	No	
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	There is no existing path or route in the area. Nearest sidewalk is 1500' away at the park city hotel. The hotel is serviced by the SR-224 stop. Connect the two stops via sidewalk to the hotel?
		Accessible route to streets, sidewalks, and pedestrian paths		
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)		
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)		
Width	$\geq 36''$ (exception: may be reduced to 32'' minimum for a length of 24'' provided reduced width segments are separated by segments that are a minimum of 48'' long by 35'' wide)			
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	None

Other Comments: No sidewalks or routes nearby, are we assuming they are getting dropped off here and picked up? 12' from TBC to fence line. 33-36% slope from TBC to fence. Vegetation overhang as well that will need be cleared. Room for sidewalk on east side between fence and TBC. No room for bus pull out.

SR-224 AND PAYDAY DRIVE



- 1 ADD CROSS WALK
- 2 PROPOSED BUS SHELTER, PAD, BENCH
- 3 NEW BUS PULL OUT AREA
- 4 NEW 5' SIDEWALK
- 5 EXISTING CREEK/POND
- 6 EXISTING TRAIL
- 7 ADD PAD AND BENCH AT WESTERN STOP AREA.
- 8 EXISTING SIGNAL POLE

CRITICAL ELEMENTS:

- WESTERN STOP IS SPACE LIMITED. LAND OWNERSHIP MAY BE AN ISSUE.
- COORDINATION WITH UDOT FOR BUS PULL OUT.
- AVOIDANCE OF CREEK AND POND.
- MID BLOCK CROSSWALK VISIBILITY

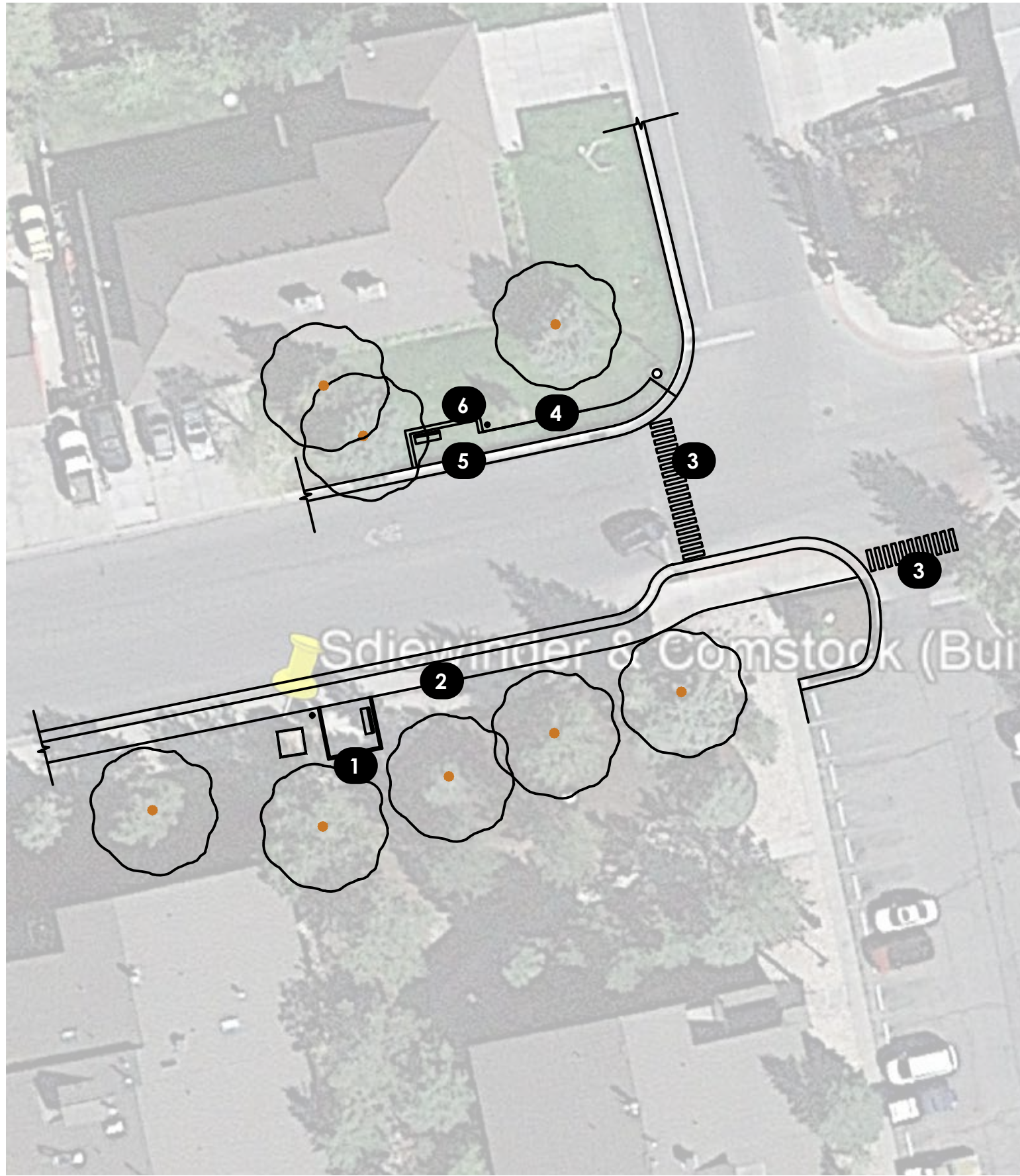
Stop Location: SR-224 and Payday Drive

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	Informal area
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	Yes	Informal area
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	Slope is mostly flat
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	Slope is mostly flat
	Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	No	Existing gravel/roadway shoulder and landscape.
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No shelter
		Connected by an accessible route to boarding and alighting area	No	No shelter or route
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No route, need connection to prospector drive stop to the west.
		Accessible route to streets, sidewalks, and pedestrian paths	No	No route to adjacent intersection
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Yes	Slope is good, just needs pavement
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Yes	Slope is good, just needs pavement
	Width	$\geq 36''$ (exception: may be reduced to $32''$ minimum for a length of $24''$ provided reduced width segments are separated by segments that are a minimum of $48''$ long by $35''$ wide)	Yes	Plenty of space for a route
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	No	No bench

Other Comments: Plenty of space, open turf grass area with 1-2% slopes. No connection to intersection or trail. Could connect back to intersection or could go due west and cross stream and tie into trail. Trail is 2-6% the entire way with mostly 3% grade. Ramp down to crossing hits 9% probably to meet the rim elevation of the manhole. All cross walks are striped. What about connection to upper stop on prospector? Lots of vegetation and bad visibility for a crossing?

compliance form/field notes

SIDEWINDER AND COMSTOCK
BUILDING 7



- 1 EXISTING PAD, BENCH, ETC. ADD SHELTER
- 2 EXISTING SIDEWALK AND BUS PULL OUT AREA
- 3 STRIPE CROSS WALKS AND ADD ADA RAMPS WITH DETECTABLE WARNING.
- 4 NEW 5' SIDEWALK
- 5 NEW BUS STOP PAD WITH BENCH
- 6 RETAINING WALL FOR NEW STOP AREA

CRITICAL ELEMENTS:

- NORTH STOP IS SPACE LIMITED AND HAS LAND OWNERSHIP ISSUES.
- CROSSWALKS ARE RAISED BUT NOT STRIPED.

Stop Location: Sidewinder and Comstock (south side of road)

Stop Element	Category	Standard	Compliant	Notes
Boarding and Alighting Area	Dimensions	Parallel to roadway: $\geq 60''$	Yes	
		Perpendicular to roadway (From curb or road edge): $\geq 96''$	Yes	
	Slope	Parallel to the roadway: same as the roadway to the maximum extent practicable	Yes	
		Perpendicular to the roadway: $\leq 2.0\%$ (+0.5% tolerance)	Yes	
	Surface	Stable, Firm, and Slip Resistant with no changes in level $>1/4''$	Yes	
Bus Shelter	Clear Floor/Ground Space	$\geq 30''$ by $\geq 48''$ entirely within shelter	No	No Shelter
		Connected by an accessible route to boarding and alighting area	No	No Shelter
Accessible Route	Connectivity	Accessible route between all bus stops within site and accessible entrance	No	No route to route on north side of road
		Accessible route to streets, sidewalks, and pedestrian paths	Yes	
	Slope	Parallel to roadway: $\leq 1:20$ (5%) (+1.0% tolerance)	Partial	No route to route on north side of road
		Perpendicular to roadway: $\leq 1:48$ (2.1%) (+0.5% tolerance)	Partial	No route to route on north side of road
	Width	$\geq 36''$ (exception: may be reduced to $32''$ minimum for a length of $24''$ provided reduced width segments are separated by segments that are a minimum of $48''$ long by $35''$ wide)	Partial	No route to route on north side of road
General	Bench	Top of the bench seat surface $\geq 17''$ and $\leq 19''$ above the finish floor or ground	Yes	Need to bolt down in permanent location

Other Comments: Stop on south side of sidewinder meets all criteria but has no shelter, pad is 11' x 12'. Bench is currently movable and could be arranged and permanently mounted to improve ADA accessibility on pad. Has rolled gutter. Stop on north side of sidewinder has nothing and is space limited. Large existing residential tree in the way. Intersection at comstock and sidewinder has a raised crosswalk but it isn't striped. Ramps on the south side meet code (4-5%) but there is no sidewalk or ramp on the north side. Roadway and adjacent lot slopes on north side range between 12-14%

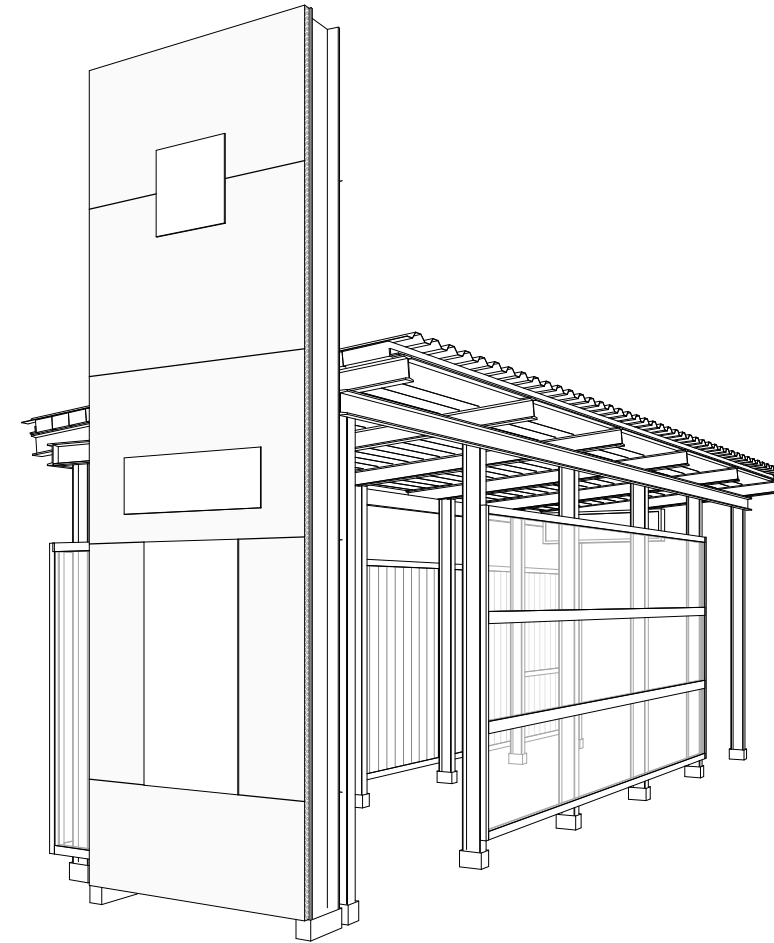
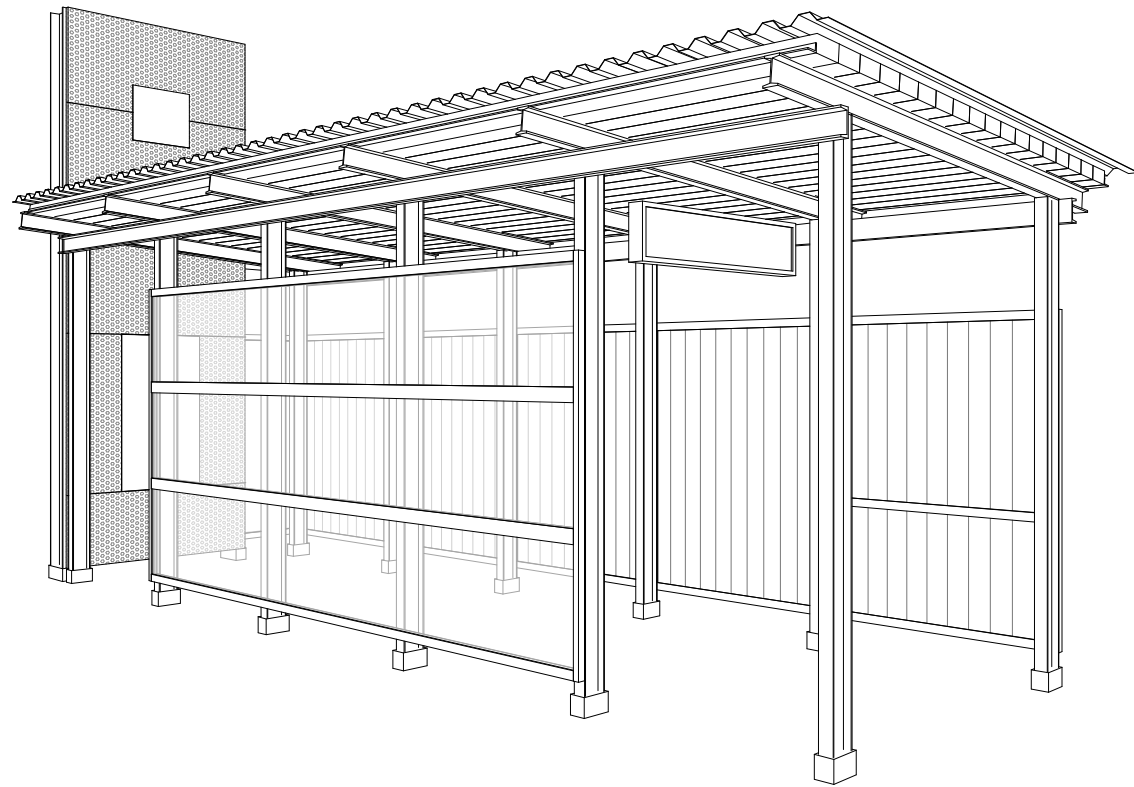
Date: 6-29-2021

ITEM NO.	DESCRIPTION	UNIT PRICE	UNITS	Three Kings & Thaynes Canyon		Holiday Village Apartments Bus Stop		Sidewinder & Comstock Building 7		2001 Park Avenue		Park City Clinic on Kearns		1378 Park Avenue at City Park		Prospector Condos on Prospector Avenue		SR 224 & Payday Drive		Comstock and Kearns		Park City High School (south side)	
				QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT	QUANTITY (Multiplier)	TOTAL AMOUNT
1	Mobilization/Demobilization	\$12,000.00	Lump Sum	1	\$12,000.00	1	\$ 12,000.00	1	\$ 12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00
2	Traffic Control	Varies	Lump Sum	1	\$3,600.00	1	\$ 6,000.00	1	\$ 4,800.00	1	\$8,400.00	1	\$8,400.00	1	\$8,400.00	1	\$4,800.00	1	\$8,400.00	1	\$8,400.00	1	\$8,400.00
3	Sawcut and Removal of existing roadway and sidewalk	\$20.00	Square Foot	363	\$7,260.00	550	\$11,000.00	0	\$0.00	363	\$7,260.00	600	\$12,000.00	3200	\$64,000.00	430	\$8,600.00	500	\$10,000.00	1000	\$20,000.00	500	\$10,000.00
4	Asphalt Pavement	\$200.00	Ton	42	\$8,400.00	14	\$2,800.00	0	\$0.00	42	\$8,400.00	37	\$7,400.00	30	\$6,000.00	11	\$2,200.00	31	\$6,200.00	62	\$12,400.00	26	\$5,200.00
5	Untreated Base Course Under Asphalt	\$68.00	Ton	84	\$5,712.00	28	\$1,904.00	0	\$0.00	84	\$5,712.00	74	\$5,032.00	60	\$4,080.00	22	\$1,496.00	62	\$4,216.00	124	\$8,432.00	52	\$3,536.00
6	Concrete Curb and Gutter	\$28.00	Linear Foot	272	\$7,616.00	220	\$6,160.00	0	\$0.00	272	\$7,616.00	240	\$6,720.00	400	\$11,200.00	172	\$4,816.00	200	\$5,600.00	400	\$11,200.00	200	\$5,600.00
7	Concrete Sidewalk (4' Wide)	\$20.00	Linear Foot	320	\$6,400.00	210	\$4,200.00	262	\$5,240.00	320	\$6,400.00	278	\$5,560.00	375	\$7,500.00	215	\$4,300.00	500	\$10,000.00	62	\$1,240.00	214	\$4,280.00
8	Retaining Wall (Park City Standard - Corten Steel Panel with Timber Posts)	\$170.00	Square Foot	160	\$27,200.00	30	\$5,100.00	100	\$17,000.00	160	\$27,200.00	0	\$0.00	0	\$0.00	160	\$27,200.00	0	\$0.00	180	\$30,600.00	0	\$0.00
9	6' Prefabricated Bench	\$1,200.00	Each	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00	1	\$1,200.00
10	Concrete Pad for Bus Stop (Approximately 8' x 16')	\$2,560.00	Each	1	\$2,560.00	1	\$2,560.00	1	\$2,560.00	1	\$2,560.00	1	\$2,560.00	2	\$5,120.00	1	\$2,560.00	1	\$2,560.00	1	\$2,560.00	1	\$2,560.00
11	Reinforced Concrete Bus Pull Out Area (Approximately 12' x 60')	\$21,600.00	Each	0	\$0.00	1	\$21,600.00	0	\$0.00	1	\$21,600.00	1	\$21,600.00	0	\$0.00	0	\$0.00	1	\$21,600.00	1	\$21,600.00	1	\$21,600.00
11	Park City Standard 3 Sided Shelter (Approximately 7' x 15')	\$25,000.00	Lump Sum	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00	1	\$25,000.00
12	Roadway Excavation	\$11.00	Cubic Yard	363	\$3,993.00	122	\$1,342.00	0	\$0.00	363	\$3,993.00	320	\$3,520.00	300	\$3,300.00	2723	\$29,953.00	267	\$2,937.00	533	\$5,863.00	222	\$2,442.00
13	ADA Ramps with Truncated Domes	\$5,000.00	Each	1	\$5,000.00	2	\$10,000.00	2	\$10,000.00	1	\$5,000.00	2	\$10,000.00	4	\$20,000.00	2	\$10,000.00	1	\$5,000.00	2	\$10,000.00	2	\$10,000.00
14	Signing	\$500.00	Each	2	\$1,000.00	2	\$1,000.00	2	\$1,000.00	2	\$1,000.00	2	\$1,000.00	3	\$1,500.00	2	\$1,000.00	2	\$1,000.00	2	\$1,000.00	2	\$1,000.00
15	Removal of existing curb and gutter	\$6.00	Linear Foot	\$ -	\$ -	\$ 100.00	\$ 600.00	\$ -	\$ -	\$ -	\$ -	\$0.00	\$0.00	\$ 400.00	\$ 2,400.00	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -
16	Landscape - Includes Removal/Demo of Existing and Restoration to Original Condition or Better	\$8.00	Square Foot	600	\$4,800.00	3000	\$24,000.00	1000	\$8,000.00	1000	\$8,000.00	1000	\$8,000.00	2500	\$20,000.00	1000	\$8,000.00	2000	\$16,000.00	1000	\$8,000.00		\$0.00
17	Utility Relocation and/or Improvements	Varies	Lump Sum	1	\$1,000.00	1	\$ 10,000.00	1	\$ 10,000.00	1	\$20,000.00	1	\$30,000.00	1	\$1,000.00	1	\$1,000.00	1	\$5,000.00	1	\$5,000.00	1	\$1,000.00
18	VMS Board Power Connection to RMP Transformer and Conduit Stubs	\$12,000.00	Lump Sum	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00	1	\$12,000.00
19	Cross Walk Striping/Restriping	\$1,000.00	Each	1	\$1,000.00	1	\$1,000.00	2	\$2,000.00	0	\$0.00	1	\$1,000.00	3	\$3,000.00	0	\$0.00	0	\$0.00	0	\$0.00	2	\$2,000.00
Construction Total				\$135,741.00	\$159,466.00	\$110,800.00	\$183,341.00	\$172,992.00	\$207,700.00	\$156,125.00	\$148,713.00	\$196,495.00	\$127,818.00										
Survey, Engineering, and Construction Management - 10%				\$13,574.10	\$15,946.60	\$11,080.00	\$18,334.10	\$17,299.20	\$20,770.00	\$15,612.50	\$14,871.30	\$19,649.50	\$12,781.80										
Planning Level Contingency - 15%				\$20,361.15	\$23,919.90	\$16,620.00	\$27,501.15	\$25,948.80	\$31,155.00	\$23,418.75	\$22,306.95	\$29,474.25	\$19,172.70										
Total				\$169,676.25	\$199,332.50	\$138,500.00	\$229,176.25	\$216,240.00	\$259,625.00	\$195,156.25	\$185,891.25	\$245,618.75	\$159,772.50										

preliminary cost estimate

BUS STOP ACCESSIBILITY IMPROVEMENT PROJECTS





1 | **3D View 2**
A280 | Scale:

2 | **3D View 3**
A280 | Scale:

DRAFT SET
NOT FOR CONSTRUCTION

REV	DATE	DESCRIPTION

SUBMITTAL

60% PLANS

SEAL

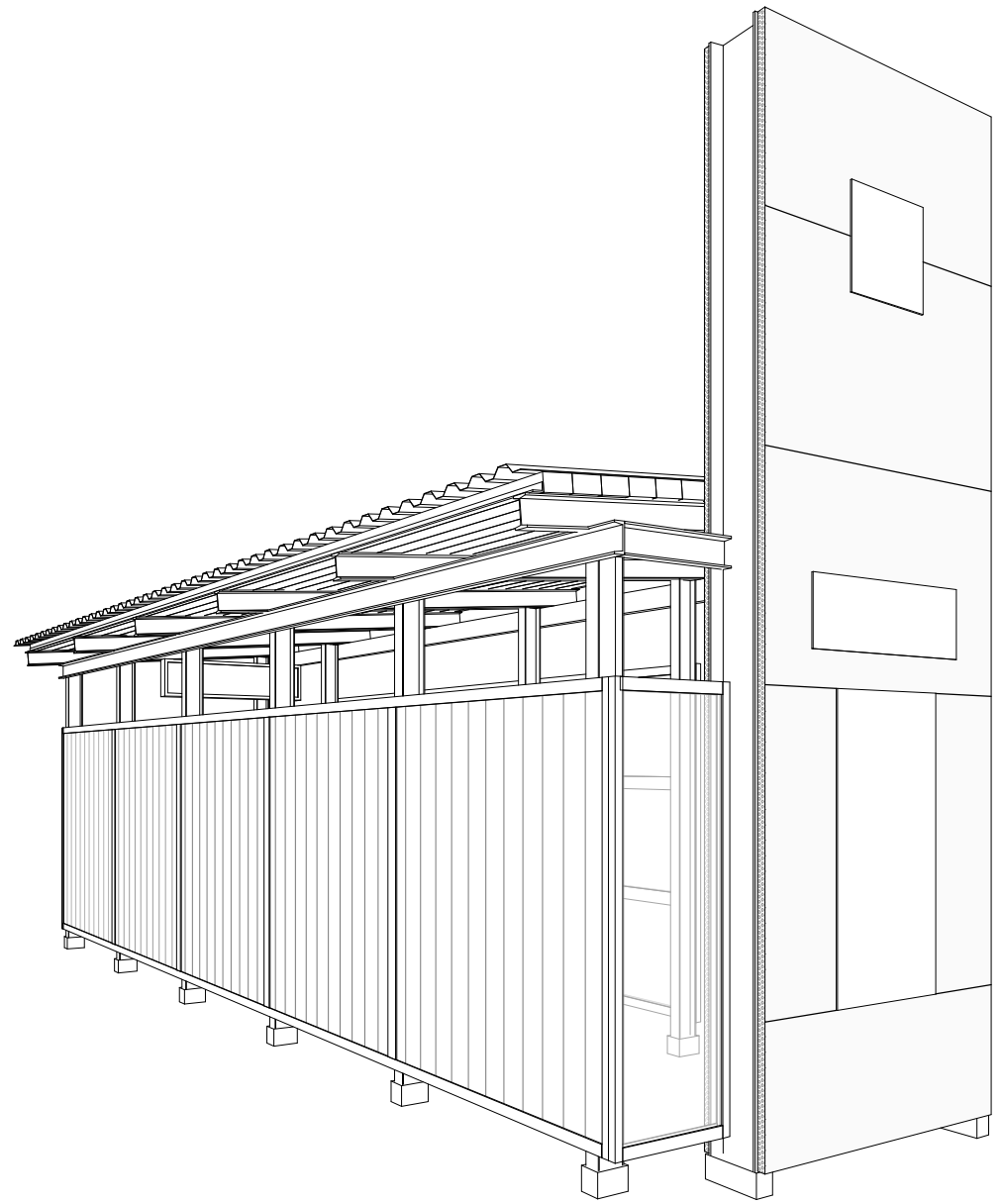
CONSULTANT
AECOM
756 East Winchester Street
Suite 400
Salt Lake City, UT 84107
Phone: (801) 904-4000
Fax: (801) 904-4100

CLIENT

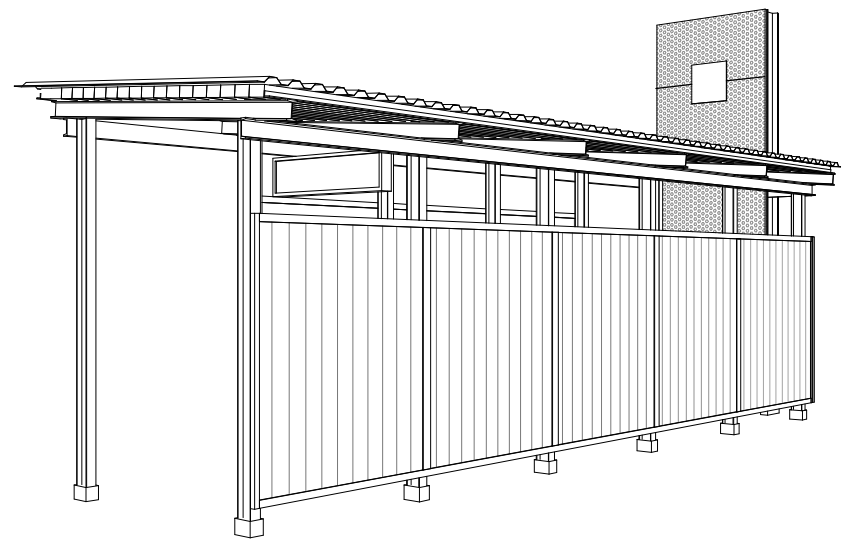

Designed By:
Park City
Drawn By:
E. Wentling
Checked By:
J. Doyle
Approved By:
J. Jacobson

PARK CITY SHELTER IMPROVEMENTS

Submittal Date:
Contract No:
EXTERIOR PERSPECTIVES
Alignment: | Sheet No:
A280



1 | **3D View 4**
A281 | Scale:



2 | **3D View 5**
A281 | Scale:

**DRAFT SET
NOT FOR CONSTRUCTION**

REV	DATE	DESCRIPTION

SUBMITTAL

60% PLANS

SEAL

CONSULTANT
AECOM
756 East Winchester Street
Suite 400
Salt Lake City, UT 84107
Phone: (801) 904-4000
Fax: (801) 904-4100

CLIENT
PARK CITY
1884


Designed By:
Park City
Drawn By:
E. Wentling
Checked By:
J. Doyle
Approved By:
J. Jacobson

PARK CITY SHELTER IMPROVEMENTS

Submittal Date:
Contract No:
**EXTERIOR
PERSPECTIVES**
Alignment: | Sheet No:
A281



1 AERIAL VIEW

			SUBMITTAL	SEAL	CONSULTANT AECOM 756 East Winchester Street Suite 400 Salt Lake City, UT 84107 Phone: (801) 904-4000 Fax: (801) 904-4100	CLIENT 	Designed By: S. HEIMSTEAD Drawn By: S. HEIMSTEAD / B. OLSON Checked By: J. DOYLE Approved By: J. JACOBSON	PARK CITY BUS SHELTER IMPROVEMENTS	Submittal Date: 12/14/2020 Contract No: 60605663
REV	DATE	DESCRIPTION							PERSPECTIVES
									Alignment: Sheet No: A-501



4 NORTHEAST VIEW



3 NORTHWEST VIEW



2 SOUTHEAST VIEW



1 SOUTHWEST VIEW

REV	DATE	DESCRIPTION

SUBMITTAL

SEAL

CONSULTANT

AECOM

756 East Winchester Street
Suite 400
Salt Lake City, UT 84107
Phone: (801) 904-4000
Fax: (801) 904-4100



Designed By:
S. HEIMSTEAD

Drawn By:
S. HEIMSTEAD / B. OLSON

Checked By:
J. DOYLE

Approved By:
J. JACOBSON

PARK CITY BUS SHELTER IMPROVEMENTS

Submittal Date: 12/14/2020

Contract No: 60605663

PERSPECTIVES

Alignment: Sheet No: A-502

PAAB Advocacy Subcommittee

PC PAAB Advocacy Subcommittee

August 30, 2022

4:00 pm - 5:30 pm

Attendees

- Lara Carlton
- Joann Askins-Stack
- Elsa Gary (host - thanks for sharing your home and art collection!)
- Dave Nicholas

Discussion

- Purpose of the meeting to define content for post card mailer as initial step in multi-tiered campaign to improve public awareness and education for locals of the PC public art collection. Also to further discuss and define second and third tier outreach strategies to follow-up post card mailer.
- Elsa shared some research findings including video titled "How to look at Public Art" narrated by a six year old boy to highlight San Francisco public art installations. [How To Look at Public Art: A Six-Year-Old Explains](#) Great example of short and engaging media approach to advocacy. Similar to current video created several years ago featuring Nann, Stephanie and Jack [Park City Public Art | Get Inspired!](#).



- Post Card Mailer
 - Committee concluded to keep initial mailer simple with a call to action, key collection statistics to peak interest and connection or links to existing public art collection website.
 - Post card mailer to 84060 residents. PAAB to provide the content to present to Board for approval. With approval, provide information to PCMC Community Engagement Team to prepare a draft post card for review and approval. Confirm process of final approval with Board and staff.

- Goal to mail post card by end of September (pending design and review requirements) to allow time for residents to get out during fall colors to experience the collection.
- Use collage image (or similar) on home page of web site to visually connect people with the mailer to the website.

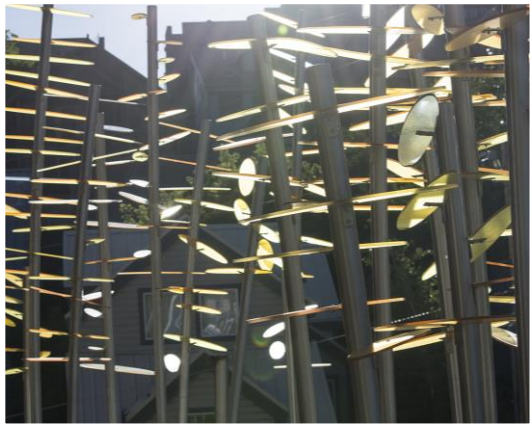


- Overlay a key message and call to action over the collage image such as "Get to know **YOUR** Park City Public Art Collection!"
 - Potentially add a hero image of a signature piece of the collection if graphic layout allows.
 - Add Quick Facts with 3-5 key stats of the collection. These could include number of total pieces in the collection, number of artists and/or states/countries contributing, number of children artists, pieces completed in the past 10 years.
 - Dave to reach out to Jenny to track down key stats that can be quantified and verified.
 - Add QR code and/or web link to PAAB site and Art Map
 - <https://www.parkcity.org/government/boards-commissions/public-art-advisory-board>
 - <https://storymaps.arcgis.com/stories/7810fb5b8f174f8daede43183466c0af>
 - Potential to start hash tag opportunity for public to hash tag their favorite pieces. This may be delayed to second tier outreach to confirm feasibility.
- Second Tier Outreach - Follow-up to initial post card mailer
 - Committee concluded that a second post card mailer would be the most efficient and cost effective way to follow-up the initial post card mailer as a second "touch" to include an announcement of a self-guided tour of the public art collection.
 - Goal to work with the Chamber and Visitors Bureau to refresh the existing tour and/or add a new one that could be focused on murals. Note...existing Chamber tour includes a few pieces that are not a part of the PCMC collection such as the Banksy.
 - Goal to send out second post card mailer within one month of the first post card mailer by end of October for repetitive "touches" (pending design and review requirements).
 - Message similar to "Get to know **YOUR** Park City Public Art Collection" for consistency with first post card mailer but further amplified with link to instructions of a self-guided tour.
 - Hash tag opportunity to start getting the public to promote the tour and to post their favorite collection piece.
 - Third Tier Outreach - Community Event
 - Committee agreed that a Spring/Early Summer event corresponding with an installation reveal such as utility boxes could provide a powerful third "touch" and a community engagement opportunity.

- Could reveal a new self-guided tour option to tour utility boxes.
- In-person event that could include contributing artists (especially kids) could be newsworthy and provide additional media coverage.

PC PAAB

PC PUBLIC ART TOUR
PROMOTIONAL POSTCARD

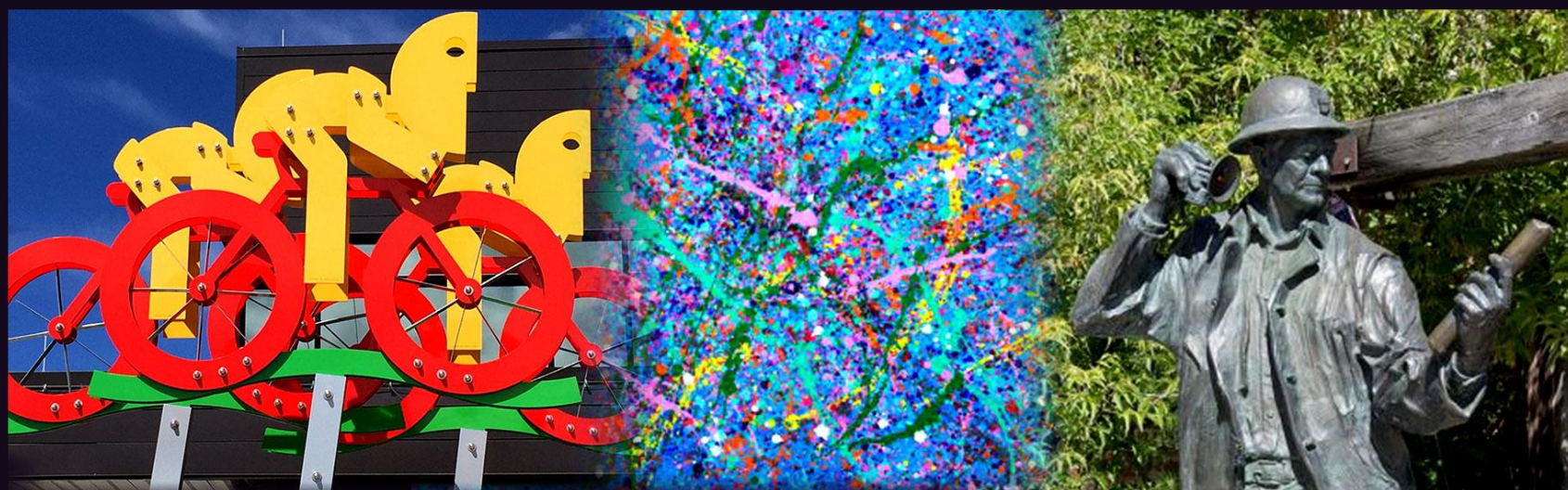


Introduction

- PAAB Subcommittee created for Awareness and Advocacy to brainstorm and prepare a potential strategy for a public outreach effort to engage the community and visitors to promote the public art collection
 - Create a promotional campaign in a thoughtful and strategic series of mailings (postcard), events, and tour options
 - This presentation's focus is the postcard portion
 - Utilize a welcoming and inclusively designed postcard

LOOKING FORWARD • LOOKING BACK

VIBRANT



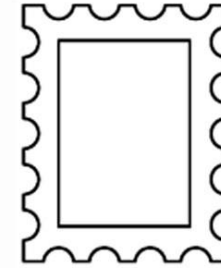
A CURRENT OF CREATIVITY RUNS THROUGH TOWN

INCLUSIVE

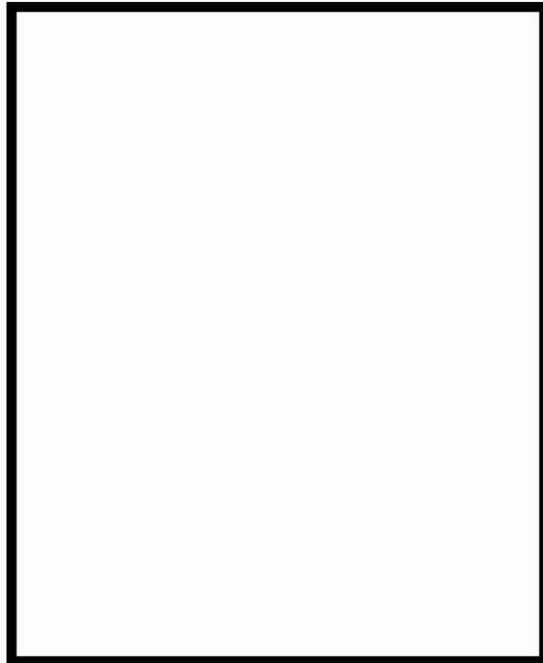


ECLECTIC

PARK CITY PUBLIC ART WALKING TOUR



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Appendix

Additional promotional thoughts

- Greeting Tour links for a PC art mural that people can see upon entering Park City.
 - <https://www.greetingstour.com/>
 - <https://www.greetingstour.com/testimonials>
 - <https://www.greetingstour.com/grant>
- [OTOCAST](#) Potential app used by cities around the world promoting local art with detailed descriptions and insights from the artists

BUDGET & PROJECT UPDATES

PAAB Budget Update January 2023

Budget Item	Amount	Notes
General Funds (CIP, Unrestricted):	\$117,090.00	3 year allocation (2015, 2016, 2017 Cp0089-031450) Combined with previous 2014 CIP Balance (Cp0089-031450), FY 2020 \$100,000 Allocation, FY 2022 \$40,000 Allocation
Lower Park RDA, Restricted	\$77,748.50	3 year allocation (FY16, 17, 18) Cp0089-03345, includes remaining Library 1% as those funds are restricted to Lower Park RDA
Total PAAB 'General Funds' Budget Balance	\$194,838.50	
1% Funding		
Deer Valley Drive II Beautification 1%	\$7,725.00	remaining in project budget.
Main Street Improvement Estimated Total 1%	\$90,000.00	Confirmed with Budget Department
Total 1% Funding	\$97,725.00	
Grant Funding		
No current grants	\$0.00	
Total Grant Funding	\$0.00	
Operational Funding	\$2,664.29	Removed marsac tree maintenance, Lucy art maintenance, DV tunnel repair, piano maintenance and mural maintenance on poison creek
Total Operational Funding	\$2,664.29	Removed restriction of deadline to spend funding. Original amount removed from CIP above
Total Public Art Budget Balance	\$295,227.79	Total Budget
Expenditures	Cost	Notes
Library Project (July 2015) 1%	\$60,251.50	July 2015 (total 1% amount was \$68,000.00)
Quinn's Public Art (September 2015) CIP	\$85,622.00	September 2015
Maintenance DV Dr. Tunnel Repair (March 2016)	\$409.71	March 2016
PC MARC Project (October 2018)	\$6,468.00	\$6,468 has been released. This was a 1% allocation from PCMARC solar renovations.
China Bridge Mural Project (October 2018)	\$40,000.00	\$40,000 has been released - removed from General Funds above.
Maintenance Lucy Moosey (July 2018)	\$200.00	July 2018 - removed from Operational Funding. Estimated additional costs that will be removed to place Lucy are estimated at \$1,000.
Window Display - Olympic Legacy	\$17,000.00	Project Completed, funding removed from grant projects
Concrete Pad for Lucy Installation	\$2,000.00	Removed from Capital Funds
Maintenance Marsac Art Poles/Trees (July 2018)	\$426.00	June 2018 - removed from Operational Funding
Utility Box Project	\$22,665.63	\$3,600 has been released to artists and \$19,065.63 to the printing services contractor - removed from General Funds
Creekside Water Plant Public Art	\$26,500.00	\$19,000 from Public Art Capital Budget, \$6,500 from Water Dept
McPolin Tunnel Mural	\$10,000.00	\$10,000 has been released to artist and Removed from CIP funds above
Transit Center Tunnel	\$10,000.00	From Capital Budget
Utility Box Artwork Framing	\$3,000.00	Paid to frame works from Utility Box Artwork to display in City Hall
Maintenance Poison Creek Murals	\$2,800.00	removed from Operational funding above.
Chuck Landvatter, Mural Maintenance	\$500.00	removed vandalism from poison creek mural
Inventory	\$10,000.00	Project began, Minda to update funding released. Removed from General Fund.
Art District Fencing Artwork	\$2,400.00	from Capital Budget
Inventory Retainer	\$5,000.00	Paid \$850 based on current work so far.
Martinez Sculpture Site Prep	\$5,000.00	from Lower Park RDA funds.
Lucy the Moose Repairs	\$500.00	from Capital Budget
Utility Box Artwork and Printing	\$43,000.00	estimated - capital budget
Mural Repair	\$4,250.00	Ben & Chucks Murals. Graffiti sealer on Chuck's mural
Project Manager for Public Art	\$40,000.00	Estimated - hire project manager
Mural Repair	\$6,600.00	Estimated for mural & martinez sculpture repairs
FOSMML Daly West Sculpture	\$60,000.00	Estimated cost of art work
Total Expenditures	\$464,592.84	

PAAB Project Updates 1/9/23

2023 PAAB Meeting Dates:

January 9, February 13, March 13, April 17*, May 8, June 12, July 10, August 14, September 11, October 16*, November 13, December 11

Please continue to refer to the [PAAB's Mission, Vision and Goals](#).

Please also continue to refer to the [2022 Strategic Plan](#).

Maintenance and Repairs (this is a running list so when we do inventory, we can address)

- Surroundings kids have drawn on the artwork. Working to look at methods to clean the work.
- School of Fish – welding needed – artist has been contacted. Will advise on timing when weather makes it possible.

Signage & Inventory:

Working on sign installation plan for all artwork this summer. Met with Archivist and also working to include Historic Preservation Collection on 2nd Floor of City Hall in the Public Art Map and signage. Updating 3 artworks into the Public Art Map.

Projects 2023:

Utility Box Project. We have identified 40 Utility Boxes for Phase 2. Coordinating with Rocky Mountain Power. New Total Project budget is \$43,000 (Artwork is \$450/artist. Installation of artwork is estimated \$25,000).

Neighborhood Project. Budget is \$5,000

Daly West Sculpture on Rail Trail. Donated metal from Daly West Headframe. Budget is \$60,000.

Our Elks Donation – install in Spring. No cost to PAAB.

Postcard and Video Outreach in spring. Budget is \$5,000.

Neighborhood Bus Shelters: Update in December.

Link to Council Staff Communication Report for September 15 on locations is here.

<https://d3n9y02raazwpg.cloudfront.net/parkcity/e3aac8a8-41a7-11ec-a798-0050569183fa-01133467-6d34-44a8-a801-0746aa501208-1662587166.pdf> (page 109). Budget to be discussed.

Intersection Art: Update in January. Budget to be discussed.